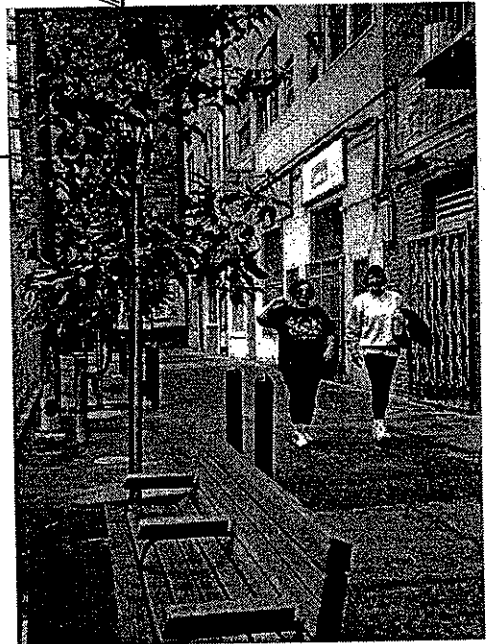

Chinatown Alleyway Master Plan

Jan. 1998



by the

Chinatown Community Development Center
with the San Francisco Department of Public Works

The *Chinatown Alleyway Master Plan (Alleyway Master Plan)* is a joint report between the Department of Public Works (DPW) and the Chinatown Community Development Center (CCDC, formerly the Chinatown Resource Center), a non-profit community development organization based in San Francisco. The *Alleyway Master Plan* establishes design and policy visions for the future renovation of thirty-one alleyways in Chinatown.

The *Alleyway Master Plan* consists of two volumes. Volume I is the contents reported in this document. Volume II is an existing conditions report and contains drawings and maps of the thirty-one alleyways. Both volumes, with Volume I also in Chinese, are available for public review at CCDC.

The source of funds for the *Alleyway Master Plan* is the Trammel Crow (gas tax) funds. The Board of Supervisors earmarked approximately 2.3 million dollars in 1994 as capital improvement monies for the Chinatown alleyway renovation projects.

CCDC, as the lead organization and the primary sponsor, authored the *Alleyway Master Plan*, documenting a year and a half long community planning process. The contents of the report are derived from discussions and input gathered from the Chinatown community, DPW and other affected city agencies, all of whose involvement will be instrumental later in the implementation of the proposed renovation projects.

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Executive Summary

The *Alleyway Master Plan* originated from a number of years of community effort to improve Chinatown's numerous alleyways. The Chinatown Community Development Center, the Chinatown Alleyway Improvement Association and the Committee for Better Parks and Recreation in Chinatown co-founded the Chinatown Alleyway Improvement Program in 1980 to increase much needed open space opportunities within the Chinatown community. (*San Francisco's Master Plan, Objective 4, Policy 4: Aggressively continue to implement the Alleyway Improvement Program.*)

With opportunities presented anew by the recently acquired community funds, i.e. Trammel Crow funds, the *Alleyway Master Plan*, for the first time, comprehensively examines Chinatown's numerous alleyways, specifically identifying individual as well as collective alleyway needs. The *Alleyway Master Plan* contains five different "project plans:"

- (1) Renovation Plan, which outlines physical improvements;
- (2) Implementation Plan, which identifies construction phasing, budgets and schedules;
- (3) Maintenance Plan, which recommends desired policies for the maintenance of the alleys;
- (4) Enforcement Plan, which ensures the long term viability of the renovated alleyways; and
- (5) Community Education Plan, which emphasizes the importance of community involvement.

The goals of the *Alleyway Master Plan*, not unlike the overall goals of the Alleyway Improvement Program, are consistent with the San Francisco's Master Plan (see page 120 - References):

Goal 1: Recognize Chinatown's alleyways as a flexible system of open space - adaptable as needed for passive or active recreation.

Urban Design Element, Objective 4, Policy 11

Make use of street space and other unused public areas for recreation.

Chinatown Area Plan, Objective 4, Policy 4

Expand open space opportunities.

Executive Summary

Goal 2: Development of a secondary pedestrian network linking together major community activity centers.

Transportation Element, Objective 7, Policy 4

Partially or wholly close certain streets not required as traffic carriers for pedestrian use or open space.

Urban Design Element, Objective 4, Policy 13

Improve pedestrian areas by providing human scale and interest.

Goal 3: Recognize Chinatown's alleyways as community assets and develop a supportive infrastructure to raise and maintain the quality of the alleyway environments.

Urban Design Element, Objective 1, Policy 8

Increase the visibility of major destination areas and other points for orientation.

Urban Design Element, Objective 2, Policy 7

Recognize and protect outstanding and unique areas that contribute in an extraordinary degree to San Francisco's visual form and character.

Urban Design Element, Objective 4, Policy 3

Provide adequate lighting in public areas.

Urban Design Element, Objective 4, Policy 5

Provide adequate maintenance for public areas.

Urban Design Element, Objective 4, Policy 14

Remove and obscure distracting and cluttering elements (i.e. overhead wires.)

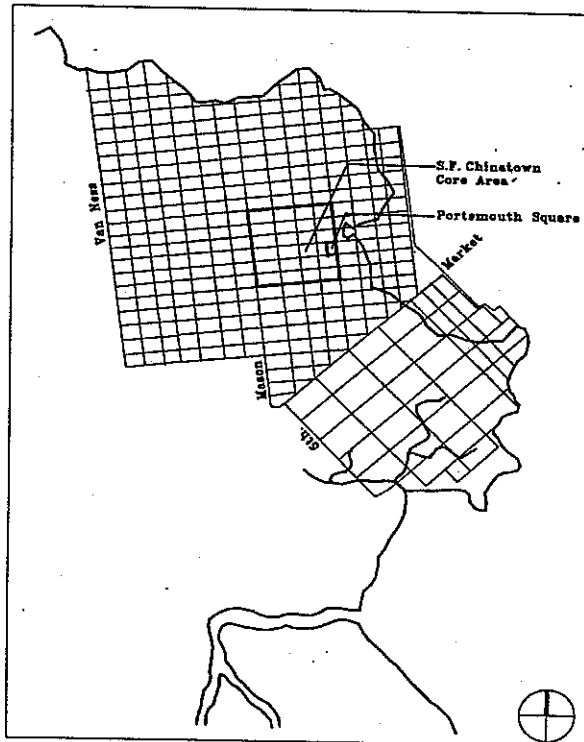
Chinatown Area Plan, Objective 1, Policy 4

Protect the historic and aesthetic resources of Chinatown.

Goal 4: Establishment of a community planning and implementation process for those who live, work, own properties and/or socialize in Chinatown.

A. Background

San Francisco's Chinatown is a thriving community of densely populated residential and commercial activities. One of the attractions that draw many people to Chinatown is the fact that it is a neighborhood, a place where the vitality is created by the presence of people living, working and playing there twenty-four hours a day. While locals visit for more familiar errands, tourists come to Chinatown to experience this unique quality. Not surprisingly, Chinatown is the second most popular tourist destination in the entire city.



San Francisco's Chinatown

Streets and alleyways of Chinatown are where the life of Chinatown can be best observed and experienced. In the main streets, i.e. Grant and Stockton, the most apparent activities are those generated by businesses and shops. In the smaller side streets, the alleyways, it is a different environment. The alleyways are where the locals frequent; they are less familiar areas for tourists and have a residential character that is atypical of what one would think of an "alleyway" or a "back street" to be. Alleys, along with the main streets, make Chinatown what it has been and what it is today, a close-knit community of businesses and residents.

The focus of this report is alleyways. Because of their unique function in Chinatown, serving not only as utility lanes but as extended living rooms for a densely packed community, the improvement and the enhancement of Chinatown's alleyways become increasingly important. The aged nature of Chinatown is another factor why alleyways should now be reevaluated. Although they are very old, alleys historically have received very little or no attention in regards to maintenance and renovation. Now with an opportunity presented anew by the *Alleyway Master Plan*, Chinatown's alleyways will receive the attention deserving of their multiple functions: a residential community, a business community and a tourist destination.

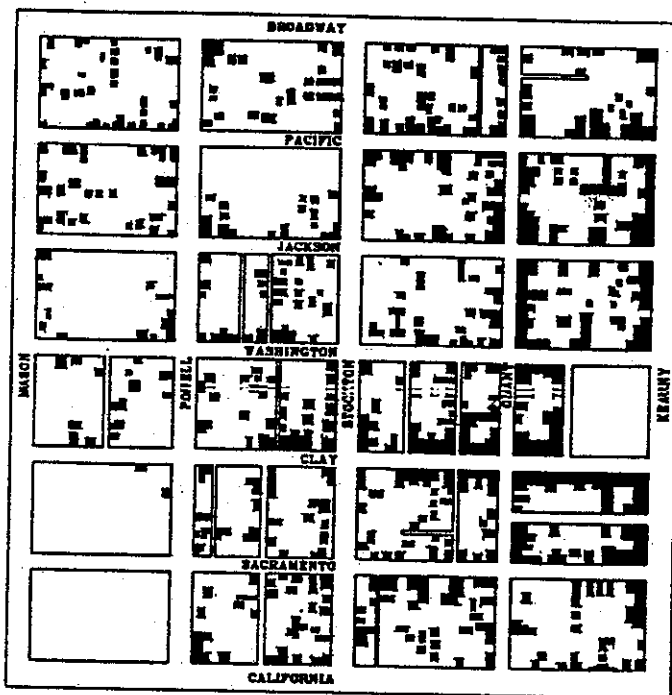
Chinatown's Alleyways

Within the Chinatown core area, which is defined in this report as a four by six block area surrounded by Broadway, Kearny, California and Mason, there are a total of forty-one alleyways. Like the rest of Chinatown, the alleyways were integral to the City's urban fabric from the earliest point in San Francisco's history.

In the mid-1850's as the city grid expanded from Portsmouth Square, San Francisco's first public plaza and now one of the most recognized landmarks in Chinatown, the alleys developed as short-cuts, in part, as a practical response to the large city blocks that resulted. They were, as they are today, pedestrian passageways. While the main streets, particularly the north-south arteries such as Grant and Stockton, functioned primarily as transportation corridors, the smaller east-west streets and alleyways were places where the daily life of the community was conducted.

Much of Chinatown alleyways' function and usage have not changed significantly since the early days. Alleys are still community living spaces but, especially today, serve additionally as places of destination, social and community building spaces, places of tourist attraction, places for business, and most significantly, much needed outdoor rooms and recreational spaces for residents. For all of these reasons, it is especially pertinent that Chinatown's alleyways are treated as more than "back streets."

More recently, it is with a community purpose in mind that Chinatown's alleyways have been viewed. A historic report commissioned by the Chinatown Community Development Center and written by architect Mui Ho, *Chinatown Alleyways - Their Potential, Their Future* (see page 120 - References), was



*Chinatown's Streets and Alleyways in 1853
(also shown are building blocks in black)*

Introduction

***"Aggressively continue to
implement the Alleyway
Improvement Program."
San Francisco's Master Plan***

completed in 1980 to explore the possibility of using the alleys as "breathing rooms" or outdoor rooms for the community. The report's findings led to a declaration in the Chinatown Area Plan of the San Francisco's Master Plan to "aggressively continue to implement the Alleyway Improvement Program (see page 120 - References.)" The *Chinatown Alleyway Master Plan* carries forward these past efforts in making sure that Chinatown's alleyways really do serve the needs of the Chinatown community and in turn, that of the City.

B. Chinatown Alleyway Master Plan

The idea for the *Chinatown Alleyway Master Plan* originated at the Chinatown Community Development Center (CCDC) in 1980 when CCDC launched the Alleyway Improvement Program. From 1981 to 1993, the Alleyway Improvement Program completed renovation work in ten alleyways within and closely adjacent to the Chinatown core area (see page 120 - References.) Although the projects were successful, a goal of the Alleyway Improvement Program had been to look at the needs of the alleyways comprehensively and at one time, so that the financial resources available could be utilized efficiently and with a greater community impact. The opportunity to reach such a goal came when city funds became available to do a project called the *Chinatown Alleyway Master Plan*.

Under the leadership of the CCDC and two other community groups, the Chinatown Alleyway Improvement Association (CAIA) and the Committee for Better Parks & Recreation in Chinatown (CBPRC), funding for the *Alleyway Master Plan* was sought for a number of years before a resolution was finally reached in 1994. The project funds, also known as the Trammel Crow Funds, are gas tax funds. Although limited to transportation related improvements (see page 80 - Implementation Plan), the Trammel Crow Funds present the community with a rare one-time opportunity to do a comprehensive study of the alleyways, thereby effectively identifying specific alleyway needs prior to actual improvements.

1. Objectives

The *Alleyway Master Plan* is a project to renovate thirty-one alleyways in San Francisco's Chinatown. Until now, a comprehensive planning and design of Chinatown's numerous alleyways have never been examined. As a holistic plan of renovation, maintenance, enforcement and community education, the *Alleyway Master Plan* highlights alleyways as community assets; cleaner, safer, more attractive and inviting alleys will no

doubt contribute significantly to revitalizing Chinatown economically, socially and physically.

The primary objective of the *Alleyway Master Plan* is to upgrade each alleyway to a level that raises the quality of the alley environments for those who live, work, visit and play in Chinatown's alleyways. It is a project that focuses on the alleys as extended living spaces and as a network of linked passages for the community, recognizing that these alleys should and are to be considered in the larger city context of continuous improvement and maintenance. The *Alleyway Master Plan* also facilitates a process by which the community can participate in the planning and design of their alleyways. This is critical, because the alleys are community spaces and whatever improvements are done should foremost reflect the needs of their primary users.

2. Functions

...recommendations set forth in the Alleyway Master Plan should provide for ways in which the renovated alleyways can be maintained and sustained to provide livable alley environments, long after the completion of the renovation projects.

The primary function of the *Alleyway Master Plan* is to guide the design, development and implementation of the renovation of Chinatown's alleyways. Additionally, the recommendations set forth in the *Alleyway Master Plan* should provide for ways in which the renovated alleyways can be maintained and sustained to provide livable alley environments, long after the completion of the renovation projects. In this respect, the *Alleyway Master Plan* is also a blueprint to initialize processes beyond the renovation work.

Because of the nature of its functions, the intended audience for the *Alleyway Master Plan* are first and foremost, the city departments, affected public agencies and community groups who will be the implementors of the plans proposed here. Secondly, the *Alleyway Master Plan* is for the Chinatown community, the primary beneficiaries, to serve as a benchmark for future expectations in regards to the alleyways.

3. Components

The *Alleyway Master Plan* is divided into five major components:

1. Renovation Plan;
2. Implementation Plan;
3. Maintenance Plan;
4. Enforcement Plan; and
5. Community Education Plan.

Together, the five plans provide for a comprehensive approach to improve, maintain, and sustain the alleyway environments for now and for the future. Each plan encourages collective efforts from the City and the Chinatown community for optimum results.

The **Renovation Plan** (see page 29) examines and recommends physical improvements for thirty-one alleyways. On a case by case basis, recommendations for the renovation work are carefully presented from both the community's and the City's perspectives.

The **Implementation Plan** (see page 77) establishes the budget, schedules, and the general scope of work for each of the projects defined under the Renovation Plan. A special focus is also given to current and future funding sources.

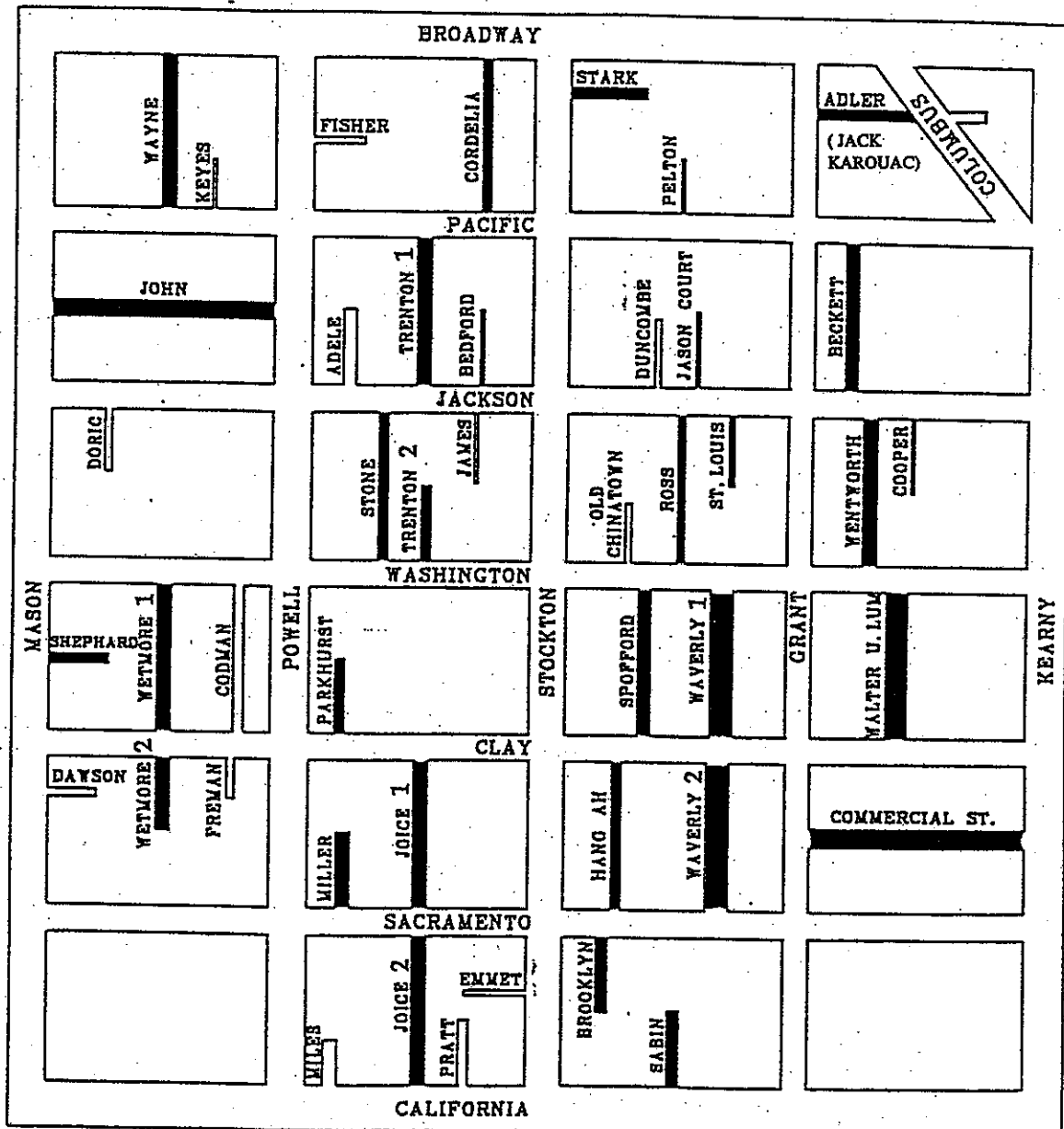
The **Maintenance Plan** (see page 83) provides guidelines for maintaining the alleys after the renovation work is completed, defining both the community's and the City's roles. Specific parameters are identified for a maintenance program(s) to be undertaken and suggestions are made on how it might be implemented.

The **Enforcement Plan** (see page 90) outlines appropriate enforcement policies to alleviate and prevent unlawful activities, i.e. illegal garbage dumping, illegal parking, in the alleys. Improving public safety and livability are particularly highlighted under the proposed enforcement program.

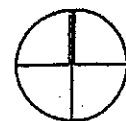
The **Community Education Plan** (see page 94) provides a set of recommendations to educate the Chinatown community on alleyway related issues. Again, the primary purpose of the education component is to aid in the continued maintenance and

Introduction

enforcement of the alleys. The Education Plan proposes methods to disseminate information within the community, to inspire and empower alleyway users to care for alleys and to inform the general public of their expectations in regards to city services.



Thirty-one Alleyways in the Alleyway Master Plan



4. Project Scope

The alleyways considered under the *Chinatown Alleyway Master Plan* are thirty-one of the forty-one total alleys located in the Chinatown core area bounded by Broadway, Kearny, California and Mason.

Because city services in regards to maintenance and enforcement are critical to the success of the renovation projects, one of the factors leading to the selection of the alleyways under study is the fact that the majority of them are both "dedicated" and "accepted" (see page 102 - Ownership and Responsibility.) All thirty-one alleyways, except Ross Alley, are dedicated or public alleys and are accepted by the City for maintenance; Ross Alley is a dedicated alley but not accepted. (A goal of the *Alleyway Master Plan* is to ensure that all thirty-one alleys are "dedicated" and "accepted" after the improvements are made.)

Other factors leading to the selection of the alleyways include (1) land use, (2) geographic context of the alleyways to the Chinatown commercial and residential core and to community facilities, and (3) the apparent intensity of pedestrian and vehicular usage.

C. The Planning Process

Because community participation is a central component to the work, the planning involved the community every step of the way.

The one and a half year planning process for the *Chinatown Alleyway Master Plan* concludes with this report. Because community participation is a central component to the work, the planning involved the community every step of the way. The Chinatown community, particularly the residents, merchants, property owners and the users of each of the thirty-one alleyways, participated in community surveys and/or meetings throughout the *Alleyway Master Plan* process.

Methodology

The project began with an analysis of existing conditions, utilizing various research methods to obtain existing information. Once the existing conditions were well defined, a needs assessment was completed through community surveys and public meetings. Together, the existing conditions analysis and the needs defined resulted in an identification of opportunities and constraints, which, together with the affected city departments' input, ultimately led to this report's recommendations.

Part I: Existing Information Collection

Existing conditions assessment was the first step. The following methods were utilized:

1. field observations;
2. field measurements;
3. background research;
4. physical site analyses;
5. community user surveys; and
6. establishment of a community database.

Field observations entailed the gathering of existing information as a preliminary basis for the *Alleyway Master Plan*. Project boundaries and project alleyways were identified, and the alleyway conditions were generally evaluated. Field observations during peak and non-peak usage hours also yielded valuable information, i.e. alley character, patterns of use, etc., which led to the

Introduction

In addition to the community user surveys, **maintenance surveys** in eleven most intensively used alleys were conducted to evaluate existing maintenance conditions (see Volume II of the *Chinatown Alleyway Master Plan*.)

Lastly, under the existing conditions assessment work, a complete **database of alleyways constituents**, i.e. residents, merchants, family associations, property owners, and social organizations of each of the thirty-one alleyways was established.

改善華埠巷里社區會議
COMMUNITY MEETING
FOR THE
IMPROVEMENT OF THE CHINATOWN ALLEYWAYS
Sponsored by the Chinatown Resource Center
由華埠改進協助社舉辦

Do you want...
• cleaner alleys!
• safer environments!
• trees in alleys!
• better lighting in alleys!

您想看見您的巷里...
• 環境更清潔嗎?
• 出入更安全嗎?
• 更舒適綠化嗎?
• 更好的照明嗎?

Come and tell us what alleyway improvements you would like to see happen in the next few years. The Chinatown Resource Center (CRC) is working on an exciting Alleyway Master Plan to improve 31 of Chinatown's alleyways and would like your important input. Everyone is welcomed to the community meetings, especially if you own property, live or work in the following alleyways:

華埠改進協助社正在策劃一項改善華埠三十一條巷里的總計劃。我們誠意邀請你出席改善華埠巷里的社區會議，提出您對華埠巷里未來工程的意見。歡迎各界人士參加，特別是在以下巷里的居民、商家、業主及雇員負責人。

應邀： 歌地利亞巷 (Cordelia)
• Bedford
• Cordelia
• John
• Stone
• Wayne
• Trenton (from Pacific to Jackson and off Washington)

應邀： 歌地利亞巷 (Cordelia)
• John
• Stone
• Wayne
• Trenton (from Pacific to Jackson and off Washington)

社區會議時間及地點是：
晚間會議
日期：二月二十八日，星期三
時間：下午五時半至七時半
地點：百老匯街七七七號
潤祥大廈
早間會議
日期：三月一日，星期五
時間：上午十一時半至一時半
地點：百老匯街七七七號
潤祥大廈

Please join us at one of the two meetings scheduled:

Evening Meeting
Wednesday, February 28, 1996
Time: 5:30 - 7:30 p.m.
Place: BaySide Elderly Housing
777 Broadway (Community Room)

Morning Meeting
Friday, March 1, 1996
Time: 11:30 - 1:30 p.m.
Place: BaySide Elderly Housing
777 Broadway (Community Room)

YOUR INPUT IS VERY IMPORTANT TO THE SUCCESS OF THE PROJECT.
請踴躍參加改善華埠巷里的社區會議。

For additional information, please call Jeremy Kow at (415) 984-1201.
欲知詳情請電：總機 (415) 984-1477 找張國明小姐 (Joni Tsui) 詢問。

A Meeting Announcement Flyer

Part II: First Round Community Meetings

Once the existing conditions were established, mapped out and evaluated, the project team conducted the first of the two community meetings under the *Alleyway Master Plan*. The primary purpose of the first round community meetings was to gather additional information directly from the stakeholders of each of the thirty-one alleyways. Similar to the community user surveys, additional needs, problems and opportunities were asked of the alleyway user.

To ensure that everyone interested was able to attend, the project team conducted two weekday meetings weekly, a lunch time and an evening meeting, for a total of four weeks. Each week, each of the four quadrants of the Chinatown core area was considered, covering about six to ten alleys at a time.

Before each of the community meetings, extensive community outreach was conducted. The project team distributed bilingual materials and meeting advertisements, conducted direct mailings and door to door outreaches, and issued press announcements of the meetings through the Chinese media.

Part III: Identification of Opportunities & Constraints and Preliminary Design Concepts

First round community meetings and the existing conditions assessment together resulted in a fairly accurate picture of

needs, identifying specific problems and opportunities for each of the thirty-one alleyways. Preliminary renovation solutions were generated at this point for further discussion.

At this stage, the thirty-one alleyways were also divided into priority categories, based on needs, opportunities and constraints identified (see page 26 - Project Priorities.) Those in the highest need categories were identified for the first group of improvements. The intermediate ones were grouped as second and the lowest needs in the third.

Part IV: City Departments' Feedback on Preliminary Concepts

Preliminary plans were presented to affected city agencies for input. The project team approached city departments who would have the most impact on the policy and implementation directions for the proposals made. For example, specific discussions on the effects of parking and traffic were addressed to the Department of Parking and Traffic. Issues related to street improvement, ownership and maintenance were discussed with the Department of Public Works. Emergency vehicle access issues were presented to the Fire Department. Based on the input received, preliminary proposals were further refined.

Part V: Second Round of Community Meetings

Once the preliminary plans for the high priority alleys were finalized after the city departments' input, the project team began the second round of community meetings for additional community feedback. A total of five meetings were held weekly to cover the first group of (nine high priority) alleys (except for Hang Ah, see page 41); most of the meetings covered one alley at a time. The meetings were held on site at the alleyways in question to ensure community participation.



A Community Meeting at Ross Alley

Part VI: Final Recommendations

Preliminary proposals for the ten priority alleys were further evaluated after the second round of community meetings. Final recommendations are made at this time with further input from the city departments.

The Final Report, the contents of this document, received final review by the City before its publication.

A. Types of Chinatown's Alleyways

Grouping alleys into common groups based on shared features, i.e. land use, size, width and orientation, reveals a lot about the types of and uses found in each of the thirty-one alleyways. The following is a summary of the alleyway types:

- north-south vs. east-west alleys;
- destination alleys vs. passageways;
- dead-ends vs. through alleys; and
- residential, commercial vs. mixed-use.

| | Types | | | | | | | | | Dimensions | | | | Ownership | | |
|-------------------------------|-------------|-----------|-------------|------------|----------|---------|-------------|------------|-----------|-------------------------|-----------------------------|--------|-------------------------|------------------------|-----------------------|---------------------------|
| | North-South | East-West | Destination | Passageway | Dead-end | Through | Residential | Commercial | Mixed-Use | Width PL/PL (ft. & in.) | Width Curb/Curb (ft. & in.) | Length | Approx. Total Area (SF) | Dedicated ³ | Accepted ³ | Not Accepted ³ |
| Alleyways | | | | | | | | | | | | | | | | |
| Beckett | X | | | X | | X | | | X | 22'-7" | 13'-4" | 275' | 6187 | X | X | |
| Bedford | X | | | | X | | | | X | 7'-6" | N/A | 137.5' | 1031 | X | X | |
| Brooklyn | X | | | X | X | | X | | | 20'-0" | 10'-2" | 137.5' | 2750 | X | X | |
| Commercial | | X | X | | | X | | X | | 35'-0" | 21'-0" | 380' | 13300 | X | X | |
| Cooper | X | | | X | X | | | | X | 5'-6" | N/A | 135' | 742 | X | X | |
| Cordelia | X | | | X | | X | | | X | 17'-6" | 11'-0" | 275' | 4812 | X | X | |
| Hang Ah (Pagoda) ¹ | X | | X | | X | | | | X | 16'-0" | N/A | 275' | 4400 | X | X | |
| Jack Kerouac (Adler) | | X | | X | | X | | | X | 17'-6" | 10'-8" | 134' | 2345 | X | X | |
| Jason Court | X | | X | | X | | | | X | 9'-5" | N/A | 138' | 1297 | X | X | |
| John | | X | X | | | X | X | | | 30'-0" | 17'-11" | 412' | 12710 | X | X | |
| Joice 1 | X | | X | | | X | | | X | 25'-6" | 12'-9"/15'-9" ² | 275' | 7012 | X | X | |
| Joice 2 | X | | X | | | X | X | | | 17'-6" | 12'-7"/10'-5" ² | 275' | 4812 | X | X | |
| Miller | X | | X | | X | | X | | | 25'-6" | 18'-8" | 138' | 3519 | X | X | |
| Parkhurst | X | | | X | X | | X | | | 17'-7" | 15'-11" | 137.5' | 2420 | X | X | |
| Pelton Place | X | | | X | X | | | X | | 6'-9" | N/A | 98' | 662 | X | X | |
| Ross | X | | X | | | X | | | X | 3'-7"/10'-10" | N/A | 275' | 3682 | X | | X |
| Sabin Place | X | | | X | X | | | X | | 21'-6" | 12'-11" | 137.5' | 2956 | X | X | |
| Shephard | | X | X | | X | | X | | | 17'-6" | 11'-7" | 110' | 1925 | X | X | |
| Spofford | X | | X | | | X | | | X | 5'-3"/14'-11" | N/A | 275' | 4152 | X | X | |
| St. Louis Place | X | | X | | X | | | | X | 10'-0" | N/A | 137.5' | 1375 | X | X | |
| Stark | | X | | X | X | | | X | | 20'-0" | 11'-4" | 137.5 | 2750 | X | X | |
| Stone | X | | X | | | X | | | X | 17'-6" | 10'-3" | 275' | 4812 | X | X | |
| Trenton 1 | X | | X | | | X | | | X | 24'-8" | 11'-8" | 275' | 6737 | X | X | |
| Trenton 2 | X | | X | | X | | | | X | 17'-4" | N/A | 137' | 2374 | X | X | |
| Walter U. Lum | X | | X | | | X | | | X | 36'-3" | 19'-10"/24-7" ² | 275' | 9969 | X | X | |
| Waverly 1 | X | | X | | | X | | | X | 41'-3" | 21'-7" | 275' | 11344 | X | X | |
| Waverly 2 | X | | X | | | X | | | X | 41'-4" | 22'-4" | 275' | 11366 | X | X | |
| Wayne | X | | X | | X | | X | | | 25'-6" | 14'-11" | 237' | 6043 | X | X | |
| Wentworth | X | | X | | | X | | X | | 26'-3" | 14'-4" | 275' | 7219 | X | X | |
| Wetmore 1 | X | | X | | | X | X | | | 25'-6" | 15'-2" | 275' | 7012 | X | X | |
| Wetmore 2 | X | | X | | X | | X | | | 25'-6" | 15'-10" | 137.5' | 3506 | X | X | |

Footnotes:

1: Data valid since Jan. 97, after the completion of the Hang Ah renovation.

2: Dimension varies throughout the alley.

3: See Appendix - Ownership and Responsibility.

Existing Conditions

North-South vs. East-West Alleys

Almost all thirty-one alleys are oriented north to south and cut across the shorter length of a typical city block. Relative to the east-west alleys, the north-south alleys have less sun exposure.

East-west alleys cut across the longer length of a typical city block, and are therefore longer than the north-south ones.

Destination Alleys vs. Passageways

Destination alleys are generally those alleys with many activities for people to go to and where a large number of front doors are found. They are typically located central to the commercial and residential cores of Chinatown and are usually mixed-use.

Passageways are those alleys which are primarily used by pedestrians as short-cuts. They serve mainly utilitarian needs and are meant to be passed through either because not too many activities are found there, and/or have very few or no front doors onto the alley. Passageway alleys are generally adjacent to sides or backs of buildings.

Dead-ends vs. Through Alleys

A number of Chinatown's alleyways are dead-ends or alleys without any through passageway for vehicles, and in some cases, for pedestrians. Dead-end alleys generally cover half a city block and are typically dead-ended by a back or a side of a building.

Through alleys are those alleys which go through the entire city block from one main street to another. They typically have a clear passageway for both pedestrians and cars.

Residential, Commercial vs. Mixed-Use Alleys

The thirty-one alleys can also be grouped under the primary land uses associated with it. Residential alleys are those alleys adjacent to primarily residential land uses. Commercial alleys are those with primarily commercial or retail uses. Mixed-used alleys have both commercial and residential uses with no one use dominant. Mixed-use alleys are also defined in this document as those alleys with institutional uses, i.e. schools, churches, etc.

Existing Conditions

In addition to the above categories of alleyway types, the thirty-one alleyways' existing physical conditions can be distinguished by the presence or the absence of certain amenities or goods, i.e. trees, street furnishings, lights, etc. These distinctions provide additional clues for future improvements. The following chart summarizes the alleyways' existing characteristics.

| | Lighting | | | | Access | | | | Garbage | | | | Vegetation | Roadway Paving | | | | | |
|-------------------------------|----------------|----------------------------|------------------------------|----------------------------|----------------|-------------------------|----------|-------------------------|-----------------------------------|-------------------|-------------------|------------------------------|-------------------------------|---|-------|----------------------------------|------------------|----------|----------------|
| | PG & E | City Standard Cobra Lights | Without Public Street Lights | Presence of Overhead Wires | Sidewalks | One Grade (No Sidewalk) | Dead-end | One-Way Through Traffic | Limited to Delivery Vehicles Only | On-Street Parking | Public Trash Cans | Commercial Garbage Dumpsters | Street Cleaning/Sweeping Sign | Warning Sign: Do Not Litter or Dump Garbage | Trees | Planters With Shrubs/Small Trees | Asphalt Concrete | Concrete | Misc. |
| Alleyways | | | | | | | | | | | | | | | | | | | |
| Beckett | X | | | X | X | | | X | | X | X | | X | X | | | X | | |
| Bedford | | | X | | | X | X | | | | | X | | | | | X | | |
| Brooklyn | | X | | | X | | X | | | | | | | | | X | X | | |
| Commercial | | X | | | X | | | X | | X | X | | X | | | | | | X ⁷ |
| Cooper | | | X | | | X | X | | | | | X | | | | | | X | |
| Cordelia | | X | | | X | | | X | | | | X | | X | | | X | | |
| Hang Ah (Pagoda) ¹ | | X | | | | X | X | | X | | | | | X | X | | | X | |
| Jack Kerouac (Adler) | | X ⁴ | | | X | | | X | | | | | | | | | X | | |
| Jason Court | | X | | | | X | X | | | | | | | | | X | | X | |
| John | X | | | X | X | | | X | | X | | | | | | | X | | |
| Joice 1 | X | | | X | X | | | X | | | | | | | X | | X | | |
| Joice 2 | X | | | X | X | | | X | | | | | | | | | X | | |
| Miller | | X ⁴ | | | X | | X | | | | | | | | | X | X | | |
| Parkhurst | | X | | | X ⁵ | | X | | | | | | | | | | X | | |
| Pelton Place | | | X | | | X | X | | | | | | | | | | | X | |
| Ross | | X | | | | X | | X | | X | | | | | | X | | X | |
| Sabin Place | | X | | | X | X | | | | | | | | | | | | X | |
| Shephard | X | | | X | X | | X | | | | | | | | | | | X | |
| Spofford | X | | | X | | X | | X | X | | | | | X | | X | X | | |
| St. Louis Place | | X ³ | | | | X | X | | | | | | | X | | X | | X | |
| Stark | | X ⁴ | | | X | | X | | | | | X | | | | | X | | |
| Stone | X | | | X | X | | | X | | | | X | | | | | X | | |
| Trenton 1 | X ² | X | | X | X | | | X | | | | X | | X | X | | X | | |
| Trenton 2 | X | | | X | X ⁶ | | X | | | | | | | | | | X | | |
| Walter U. Lum | | X | | | X | | | X | | X | | X | X | | X | | X | | |
| Waverly 1 | X | | | X | X | | | X | | X | | | | | | X | X | | |
| Waverly 2 | X | | | X | X | | | X | | X | | X | X | | X | | X | | |
| Wayne | X | | | X | X | | X | | | | | | | | | X | X | | |
| Wentworth | X | | | X | X | | | X | | X | | | X | | | | X | | |
| Wetmore 1 | X | | | X | X | | | X | | X | | | | | | | | | X ⁸ |
| Wetmore 2 | X | | | X | X | | X | | | X | | | | | | | | | X ⁸ |

Footnotes:

- 1: Data valid since Jan. 97, after the completion of the Hang Ah renovation.
- 2: PG & E pole without lights (with overhead wires only).
- 3: Light on shortened city standard pole.
- 4: Wall mounted cobra light.

- 5: Curbs only (no sidewalks).
- 6: One side only.
- 7: Brick paving with asphalt topping.
- 8: Stone paving with asphalt topping.

B. Opportunities and Constraints

Collectively or alone, each of the thirty-one alleyways possesses positive as well as negative qualities that could be either opportunities or constraints for future improvements. Field observations, personal interviews and a number of surveys conducted point to a few key conclusions: (1) Chinatown's alleyways have a great potential for improvement; (2) Chinatown's alleyways are community spaces and are well used; and (3) Chinatown's alleyways are in need of comprehensive solutions beyond physical improvements.

Opportunities

Together, the thirty-one alleyways cover approximately 3 acres or 4% of the total Chinatown core area as defined under this study. As usable open space, these figures are quite significant for the second most densely populated community in the U.S. As



Beckett Alley

such, opportunities exist for Chinatown's alleyways to serve as outdoor living rooms, recreation spaces and/or safe passageways for pedestrians. Because of their significant presence, there is also an opportunity to physically connect the alleys, serving as secondary traffic and pedestrian networks (see page 30 - Renovation Plan.)

Another positive quality that exists for Chinatown's alleyways is that they are very well utilized. This fact is most apparent in the way many people express their concerns for the alleys. In addition to those who go to alleyways daily because they live or work there, there are also people from outside of the alleys who frequent alley environments for the community facilities and shops. Additionally, children utilize some of the more pedestrian oriented alleyways, such as, Hang Ah, Ross, Spofford, and St. Louis, for recreational purposes. By the nature of the uses they attract, Chinatown's alleyways are also casual socializing spaces. People

Existing Conditions

don't hesitate to stop in an alley to rest or to converse. Many residents and merchants are typically present in the alleys at all times to monitor events, and that is most encouraging for public safety, an essential ingredient for the continued viability of all alleyways.

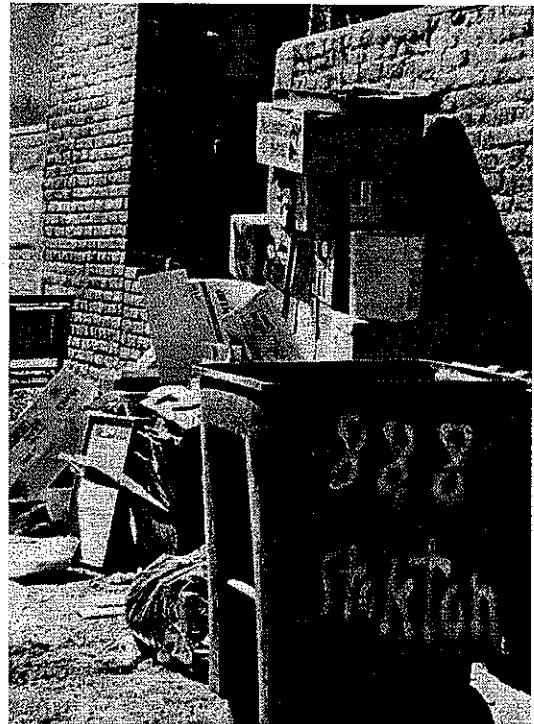
When asked about why people liked certain alleys or what they thought were positive qualities of a particular alley, many of the community survey participants came up with one or more of the following responses (also see Volume II of the *Chinatown Alleyway Master Plan*):

- convenient short-cuts for pedestrians;
- convenient for cars to go through;
- parking is "good" (space available in the alleys);
- relative to neighboring (or other) alleys, the alley in question is safer / cleaner;
- quiet / peaceful / sunny / bright / wide; and
- plants (trees).

Constraints

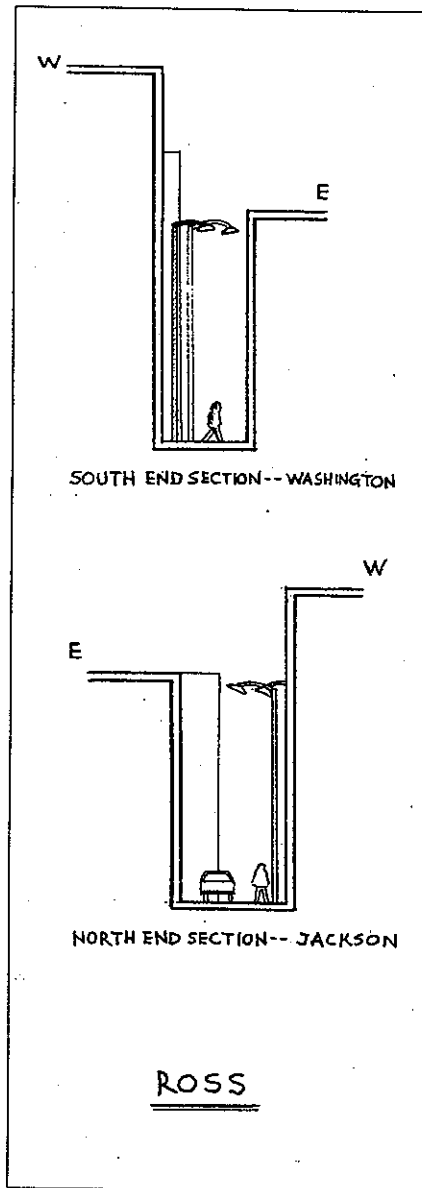
Despite the many positive qualities expressed, there are also existing constraints that must be addressed, if the alleyway renovation projects are to be successful. Generally, Chinatown's alleyways were found to have problems encompassing far more than basic physical deficiencies. Other than physical repair needs, almost all alleys suffer from (1) cleanliness (or the lack there of) issues; (2) vehicular movement conflicts with pedestrians; and (3) enforcement issues surrounding illegal parking and traffic movement that hinder the full use of the alleys.

There is no question that how and how often the alleys are cleaned affect the livability of all alleys. Aside from littering by pedestrians, cleanliness in the alleys is determined by how people contain garbage, and how and when it gets collected. Another factor, related to cleanliness, is the trash generated by others from



Garbage Conditions at Hang Ah Alley

Existing Conditions



outside of the alleyway environments; illegal dumping by residents and businesses is a major concern.

Safe and comfortable pedestrian movement in the alleyways is another important consideration. Many of the alleyways are too narrow for a comfortable pedestrian movement. For cars to safely share alley environments with pedestrians, which is unavoidable in most cases in Chinatown, a number of questions need to be addressed: (1) Are the existing alleyways wide enough to accommodate pedestrian and vehicular traffic?; (2) If not, what can be done better physically and in other ways to accommodate both?; (3) What impacts will there be for changes made to the alleys?; and (4) What are the priorities for the alleys in question, cars or pedestrians or both?.

Another pressing challenge facing the long term viability of the alley environments is how to encourage greater commitment from the community to obey laws and for the City to enforce illegal car parking and traffic movement. Many drivers have been observed to ignore traffic regulations by entering from the wrong side, entering where cars are not allowed, speeding, and by illegally parking in the alleys and on the sidewalks. These types of activities negatively impact most Chinatown's alleyways.

Not surprisingly, negative qualities of Chinatown's alleyways were not difficult to compile. The following are some of the common responses received (also see Volume II of the *Chinatown Alleyway Master Plan*):

- smelly / dirty / too much trash or garbage on street / not enough garbage cans / bad restaurant practice / rats / urine infested / bad drainage / an "old" alley, not up to date;
- too crowded (cars, people, garbage, noise);
- night time lighting is inadequate;
- safety is a concern, particularly after dark (robberies, youth gangs, people loitering, "homeless" sleeping, lack of lights);
- unsafe for pedestrians because of cars (illegally parked cars, parking in general makes certain alleys smaller or narrower, moving / speeding cars);
- lack of parking (not having enough space to park); and

Existing Conditions

- unlawful activities: double parking, parking on sidewalks, dumping garbage / unwanted items.

Recommendations - Priorities

A. Project Priorities

As alluded in the previous section, not all thirty-one alleyways will need the same types or the degree of attention as others. Because there are different needs and limited resources to fulfill those needs, the *Alleyway Master Plan* recommends that the thirty-one alleys be prioritized based on need, existing uses, and expected uses in or adjacent to the alleyway in question. The following chart summarizes the alleyways in terms of priorities.

| | Lighting | Paving & Drainage | Pedestrian &/or Vehicular Access | Cleanliness | Aesthetic Amenities | Location | | Usage | | Overall Total | Priority Rating | | |
|-------------------------------|----------|-------------------|----------------------------------|-------------|---------------------|-------------------------------------|--|------------------------|-----------------------|---------------|-----------------|-----------------------|--------------|
| | | | | | | Central to Chinatown Activities (3) | Peripheral to Chinatown Activities (1) | High Community Use (3) | Low Community Use (1) | | High Priority | Intermediate Priority | Low Priority |
| Alleyways | | | | | | | | | | | | | |
| Beckett | 2 | 2 | 3 | 2 | 3 | 3 | - | 3 | - | 18 | X | | |
| Bedford | 3 | 3 | 3 | 3 | 3 | - | 1 | - | 1 | 17 | | X | |
| Brooklyn | 2 | 1 | 2 | 1 | 3 | - | 1 | - | 1 | 11 | | | X |
| Commercial | 2 | 3 | 2 | 2 | 3 | 3 | - | 3 | - | 19 | X | | |
| Cooper | 3 | 3 | 3 | 3 | 3 | - | 1 | - | 1 | 17 | | X | |
| Cordelia | 2 | 3 | 3 | 3 | 3 | 3 | - | 3 | - | 20 | X | | |
| Hang Ah (Pagoda) ¹ | 2 | 3 | 3 | 3 | 1 | 3 | - | 3 | - | 18 | X | | |
| Jack Kerouac (Adler) | 3 | 3 | 3 | 3 | 3 | - | 1 | - | 1 | 17 | | X | |
| Jason Court | 3 | 2 | 2 | 2 | 2 | 3 | - | 3 | - | 17 | | X | |
| John | 3 | 3 | 2 | 1 | 3 | 3 | - | 3 | - | 18 | X | | |
| Joice 1 | 2 | 2 | 2 | 2 | 1 | - | 1 | 3 | - | 13 | | | X |
| Joice 2 | 2 | 1 | 2 | 1 | 2 | - | 1 | - | 1 | 10 | | | X |
| Miller | 2 | 1 | 1 | 1 | 1 | - | 1 | - | 1 | 8 | | | X |
| Parkhurst | 2 | 2 | 1 | 1 | 2 | - | 1 | - | 1 | 10 | | | X |
| Pelton Place | 2 | 1 | 2 | 2 | 2 | 3 | - | - | 1 | 13 | | | X |
| Ross | 3 | 2 | 3 | 3 | 2 | 3 | - | 3 | - | 19 | X | | |
| Sabin Place | 2 | 1 | 2 | 1 | 3 | - | 1 | - | 1 | 11 | | | X |
| Shepherd | 3 | 1 | 2 | 1 | 2 | - | 1 | - | 1 | 11 | | | X |
| Spofford | 3 | 3 | 3 | 3 | 2 | 3 | - | 3 | - | 20 | X | | |
| St. Louis Place | 2 | 2 | 2 | 3 | 2 | 3 | - | 3 | - | 17 | | X | |
| Stark | 3 | 3 | 3 | 3 | 3 | - | 1 | - | 1 | 17 | | X | |
| Stone | 3 | 2 | 2 | 2 | 3 | 3 | - | - | 1 | 16 | | X | |
| Trenton 1 | 2 | 1 | 2 | 2 | 1 | 3 | - | 3 | - | 14 | | X | |
| Trenton 2 | 2 | 2 | 3 | 2 | 3 | 3 | - | - | 1 | 16 | | X | |
| Walter U. Lum | 2 | 1 | 2 | 2 | 1 | 3 | - | 3 | - | 14 | | X | |
| Waverly 1 | 3 | 2 | 3 | 2 | 2 | 3 | - | 3 | - | 18 | X | | |
| Waverly 2 | 3 | 2 | 3 | 2 | 2 | 3 | - | 3 | - | 18 | X | | |
| Wayne | 2 | 2 | 2 | 1 | 1 | - | 1 | - | 1 | 10 | | | X |
| Wentworth | 3 | 3 | 3 | 3 | 3 | 3 | - | 3 | - | 21 | X | | |
| Wetmore 1 | 2 | 2 | 1 | 1 | 3 | - | 1 | - | 1 | 11 | | | X |
| Wetmore 2 | 2 | 3 | 2 | 1 | 3 | - | 1 | - | 1 | 13 | | | X |

Point System:

1 = good condition (unless otherwise noted)

2 = fair condition

3 = poor condition (unless otherwise noted)

Footnotes:

1: Data valid since Jan. 96, before the completion of the Hang Ah Alleyway renovation in Jan. 97.

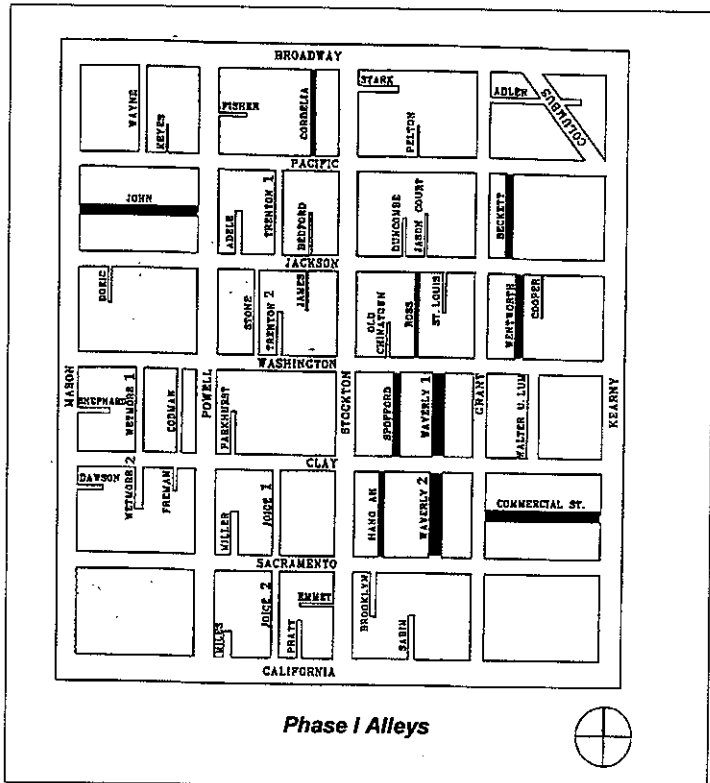
Recommendations - Priorities

For efficiency and ease of implementation, the prioritized alleys are divided into three phases of construction: Phases I, II and III (also see pages 77 to 79 - Implementation Plan.)

Phase I

Phase I alleys are prioritized as top priorities and will be, for the most part, considered for comprehensive improvements. They are also intended to be under construction first before the subsequent phases. Phase I alleys are as follows:

- Beckett;
- Commercial;
- Cordelia;
- Hang Ah (Pagoda);
- John;
- Ross;
- Spofford;
- Waverly 1;
- Waverly 2; and
- Wentworth.

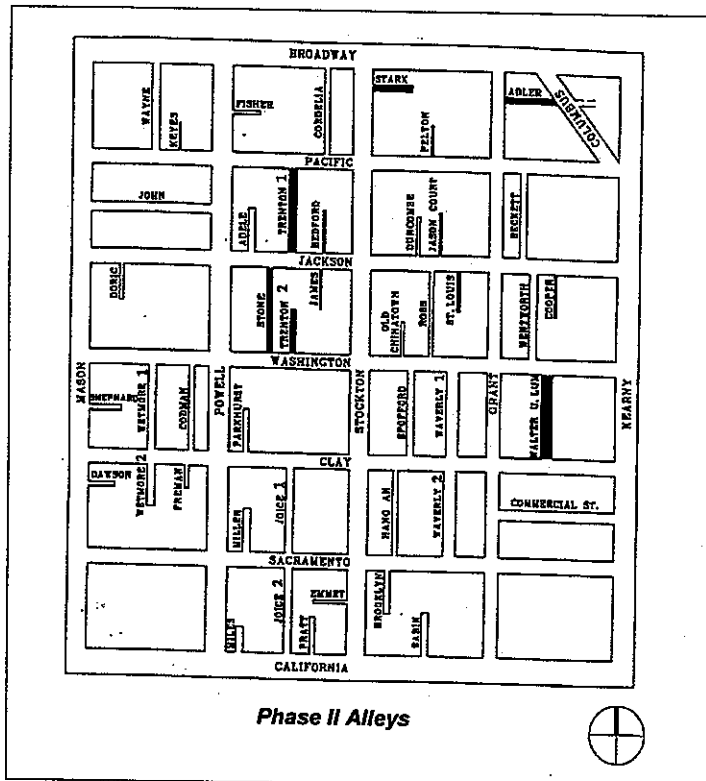


Within the ten high priority alleys, Hang Ah or Pagoda Alley was improved for the first time in 1984 and more recently in 1996. Because of the most recent renovation work, funded by Community Development Block Grant Funds, Hang Ah will not get major treatment under Phase I, but some additional improvements for it are proposed (see page 41.)

Phase II

Phase II alleys are of the second highest priority under the *Alleyway Master Plan* and should be considered for the second phase of construction. These are alleys that are mostly in fair condition with one or two outstanding issues needing the most attention. For example, some of the Phase II alleys may need better street lighting, and in others, the most outstanding improvement needed may be simply to keep the alley litter free.

Recommendations - Priorities



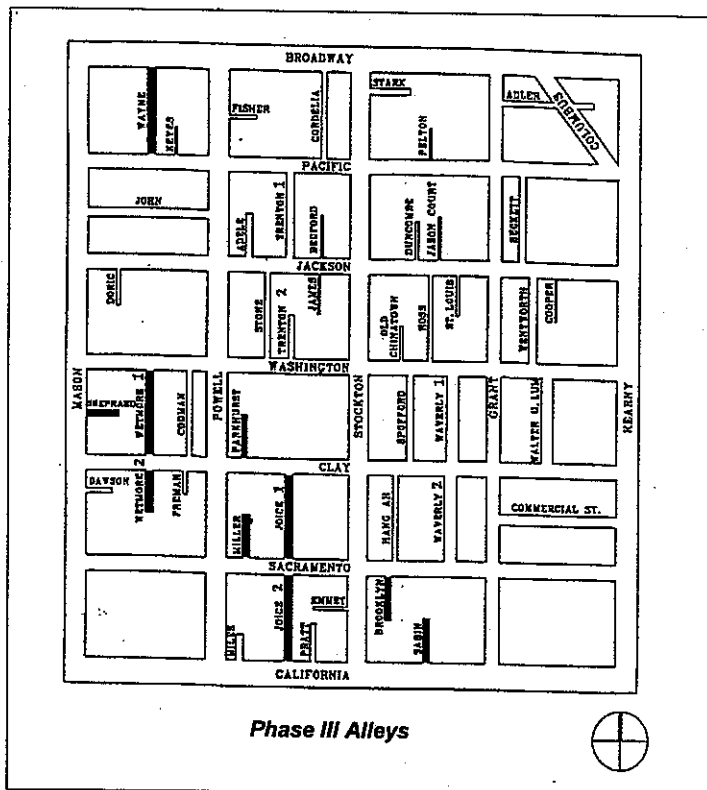
Unlike Phase I alleys, Phase II alleys do not require comprehensive changes to the alleyway environment. Phase II alleys are as follows:

- Bedford;
- Cooper;
- Jack Kerouac (Adler);
- Jason Court;
- St. Louis;
- Stark;
- Stone;
- Trenton 1;
- Trenton 2; and
- Walter U. Lum.

Phase III

Phase III alleys are in the best condition currently and need very little improvement or no improvement at all. Phase III alleys, therefore, are recommended to be the third in line for construction. Phase III alleys are as follows:

- Brooklyn;
- Joice 1;
- Joice 2;
- Miller;
- Parkhurst;
- Pelton;
- Sabin;
- Shephard;
- Wayne;
- Wetmore 1; and
- Wetmore 2.



Recommendations - Renovation

B. Project Plans

1. Renovation Plan

Aesthetically, almost all thirty-one alleyways can be defined as "beautiful." Many of the alleyways inherently have interesting and varied facades that open onto the alleys, have buildings of historic and community importance and are lively with varied activities. Given these conditions, very little needs to be or should be "beautified" in the alleys. Instead, whatever renovation proposals are made should complement and enhance each of the alleys and collectively as a group, with each improvement aimed at improving the quality and safety of the alleys for those who live, work, visit and play there.

...whatever renovation proposals are made should complement and enhance each of the alleys and collectively as a group...

The basis for the design of the thirty-one alleyways can thus be best described with two objectives:

- (1) to enhance the overall, collective image of Chinatown's alleyways; and
- (2) to achieve greater public safety, beautification and enjoyment in each of the thirty-one alleyways.

Two types of improvements are discussed here: one for the collective alleyway system under the "General Plan" and one for each alleyway under the "Specific Plans."

a. General Plan

Alleyway Networks

As discussed previously in this report, many of the thirty-one alleyways under study have natural pedestrian linkages. As a pedestrian in Chinatown, if one chooses, one could mostly use the alleyways to traverse the entire Chinatown core area in a north-south direction without ever going through the main streets. Observations and community surveys have shown that alleyways are in fact the preferred method of movement for local pedestrians familiar with the area. The *Alleyway Master Plan* is

Recommendations - Renovation

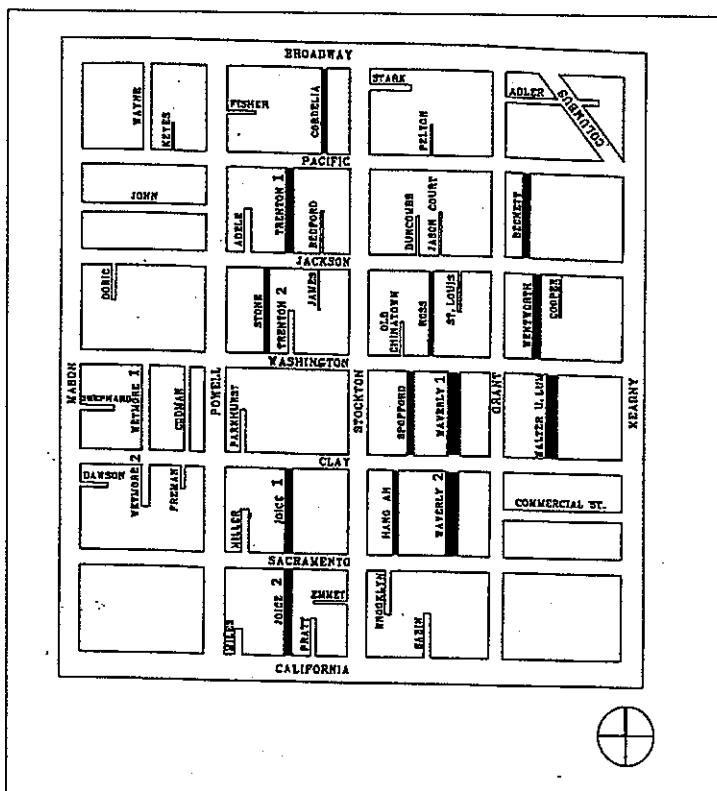
an opportunity to enhance and highlight those existing linkages.

The potential to link with other alleys also provides for some of Chinatown's alleyways to be convenient access routes for vehicles going through the Chinatown core area. Besides serving pedestrian needs, some of the alleyways are important short-cuts for vehicular traffic in a highly congested Chinatown. Where appropriate, such right-of-ways for vehicles should be preserved, while accommodating safe pedestrian movement.

Highlighting existing alleyway linkages should also help to increase the visibility of the alleys. Physically and perceptually connecting the alleys is an advantage in that the more visible and well perceived the alleys are, the more attractive the alleyways would be to locals and visitors alike. This understanding could ultimately reap economic benefits for both the City and the Chinatown community.

A number of core area alleyways have a potential to be viewed and renovated as continuous passageways. They are as follows:

- Stone, Trenton 1 and Cordelia: from south to north - Washington, Jackson, Pacific and Broadway;
- Joice 1 and Joice 2: from north to south - Clay, Sacramento and California;
- Hang Ah, Spofford and Ross: from south to north - Sacramento, Clay, Washington and Jackson;
- Waverly 1 and Waverly 2: from north to south - Washington, Clay and Sacramento; and
- Walter U. Lum, Wentworth and Beckett: from south to north - Clay, Washington, Jackson and Pacific.

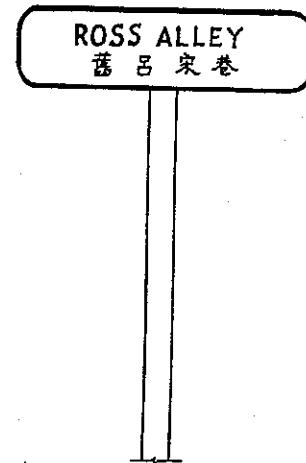


Recommendations - Renovation

General Plan Recommendations

The following recommendations are proposed overall to ensure that the alleyway linkages as they exist now are enhanced and highlighted.

- **Design Consistency:** Where appropriate, use similar materials, colors, street furnishings and signage for all thirty-one alleyways.
- **Street Name Signs:** The *Alleyway Master Plan* proposes that all thirty-one alleyways receive one design for a street sign, which is to be distinct in appearance from the main streets'. All alleyways' street signs should be in both Chinese and English text. The design is to be determined at a later date with further input from the Department of Parking and Traffic (see page 114 - Street Signage.)



In addition to the above recommendations, the following improvements are proposed for some of the alleys with natural linkages as identified above.

- **Special Art Projects:** The collective alleyway system allows for an opportunity to note the historic importance of Chinatown's alleyways. One of the ways in which this can be accomplished is through special art projects aimed to educate the alleyway user of the alleyways' historical context. Special art projects may include the following:
 - Wall murals with a theme specific to each alley;
 - The alleyway name imprinted in paving in English and Chinese text at the entrance(s) to the alley;
 - Alleyway history depicted through imprints of text in paving;
 - Special wall mounted plaques

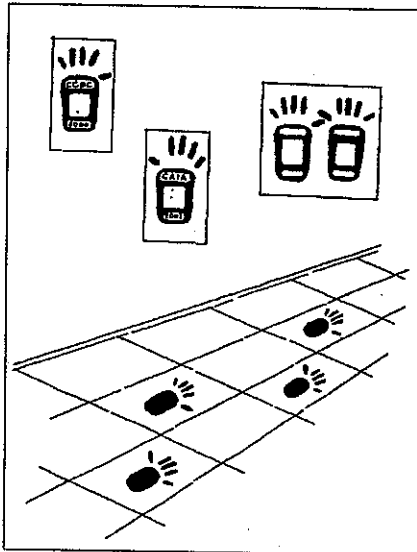


A Wall Mural at Romolo Alley, off Broadway Street in San Francisco

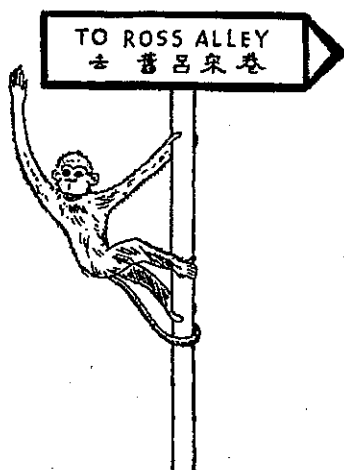
Recommendations - Renovation

identifying the alleyways' historical roles;

- Imprints of maps in paving indicating directions to alleyways in Chinatown; and
- Tiles or painting projects on vertical and horizontal surfaces, i.e. planters, walls, and paving, depicting the history of each alleyway.



- **Footprints Across Chinatown:** A monkey is an auspicious animal for the Chinese. In folk tales and Chinese operas, it is portrayed as a helper of mankind doing good for and protecting society. It is appropriate, then, that the alleyway networks are highlighted utilizing monkey footprints imprinted in the paving. This idea is very similar in concept to other projects already established, i.e., the Ribbon Project on the Embarcadero at the San Francisco Waterfront, and the Freedom (brick) Trail through Boston's historic city center, both of which are used to link one part of a city to another. Like these projects, the proposed footprints through the alleyway system will connect the alleyways that are already naturally associated. Each footprint could have within it, an imprint of individual names, names of social organizations and/or family associations affiliated with each alleyway.



- **Directional Street Signs** to the alleyways at key Chinatown street intersections: Directing more pedestrians to the alleys should aid in bringing more visibility to the centrally located system of the five groups of alleyway networks. A recommendation is made here to carry forward the monkey theme, as stated above, for the design of the directional signs. Specific locations of the signage are to be determined at a later with input from the community and the Department of Parking and Traffic (see page 114 - Street Signage.)

Note: Aesthetic and educational values aside, special art projects are opportunities for the community to develop a sense of pride and ownership of the alleys. By either through direct participation in the implementation of the projects and/or through monetary contributions, community members can become a part of the implementation process (see page 81 - Implementation Plan.)

Recommendations - Renovation

b. Specific Plans

Specific alleyway plans describe physical improvements and recommend policy visions to ensure the long term viability of those improvements. The proposals made are optimum achievements that have been evaluated as best for each of the alleyways, after having discussed the pros and cons of each proposal with the community and the affected city departments.

Note: Details, referenced in text and drawings, i.e. paving, lighting, planters and bollards, are illustrated on pages 57 to 59.

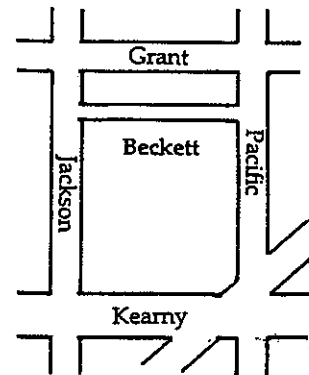
Phase I Alleys

Beckett

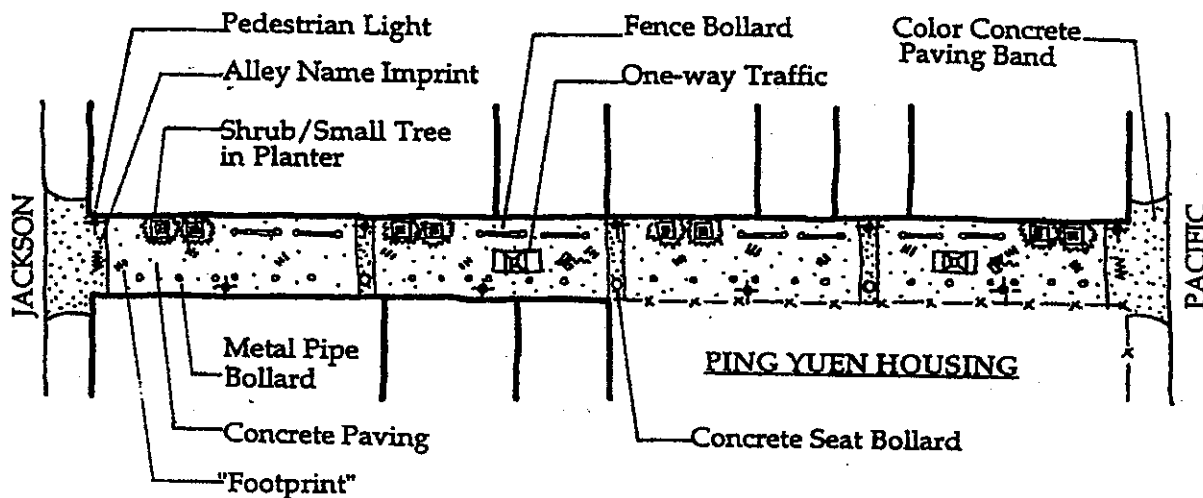
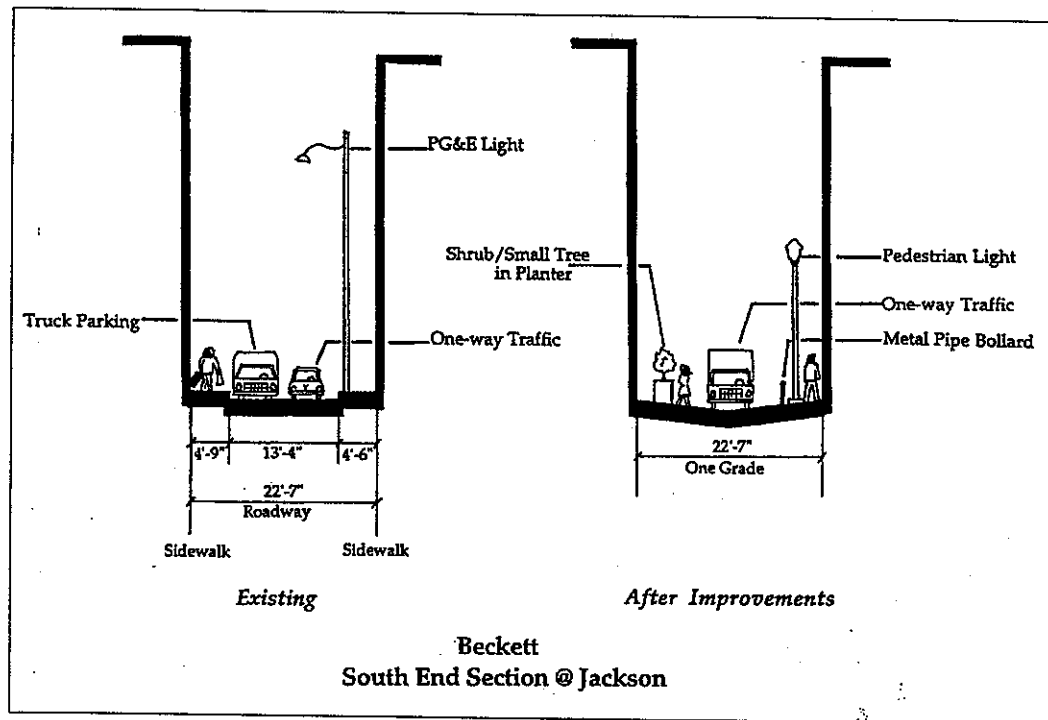
Beckett is a busy pedestrian oriented alley centrally located in the Chinatown core area. Close to Grant Avenue, it is a popular short-cut for the locals. Existing sewer lines in the alley were repaired in 1995, and consequently, the alley was resurfaced with new asphalt concrete.

Key Issues

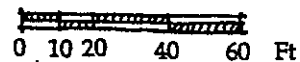
- The alley is too narrow, 13 feet and 4 inches from curb to curb, to safely accommodate both on-street truck parking and through traffic. Pedestrians walk in the street, because sidewalks are also too narrow. Existing bollards do not prevent cars from encroaching upon the sidewalks; drivers still drive dangerously close to pedestrians.
- The existing on-street truck parking serves long term users and not the two-hour truck parking users as intended. Without proper enforcement, illegal parking is common. Trucks and other vehicles occupy parking spaces at Beckett for longer than the time allowed.
- Illegal garbage dumping and littering are common; the west side of the alley, next to the parked cars, is especially dirty.
- Existing lights are PG&E wood pole lights - 3 total (changed to high pressure sodium bulbs from mercury vapor in 1994) serviced by overhead wires.



Recommendations - Renovation



Beckett



Recommendations - Renovation

Physical Improvement Proposals for Beckett

Paving

- Concrete paving. Reconstruct the entire alley.

Lighting

- City standard pedestrian lights. Remove existing PG&E lights, underground overhead wires and install new pedestrian scale lights (see page 99 - Lights.)

Landscaping

- Shrubs/small trees in planters.

Traffic

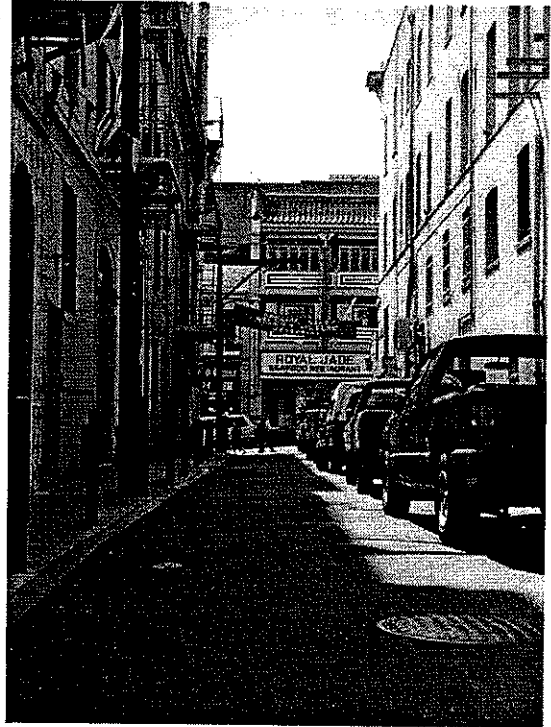
- One grade alley. Remove sidewalks and level the alley to be at one grade.
- One-way street. Allow vehicular movement one-way from Pacific to Jackson as is.
- Bollards. Utilize three types of bollards, metal pipe, concrete seat and fence, to delineate pedestrian zones from the vehicular way.
- Remove existing on-street truck parking.

Other Enhancements

- Alleyway name in English and Chinese imprinted in paving (see page 31 - General Plan.)
- Special Project: Footprints across Chinatown (see page 32 - General Plan.)
- Special Art Projects: decorative planters (see page 32 - General Plan.)

Policy Visions for Beckett

- Remove all on-site commercial garbage dumpsters. Businesses, as required by law, should contain all garbage bins on their own premises (see pages 84 to 85 - Maintenance Plan.)
- Garbage enforcement. Ensure regular enforcement of garbage and unsanitary conditions in the alley.
- Street cleaning and sweeping. Ensure regular cleaning and sweeping of the alley.
- Parking and one-way street enforcement. Ensure regular



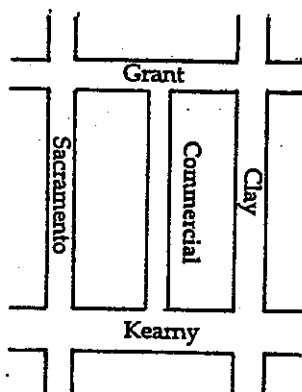
*Beckett Alley as Existing:
Looking South from Pacific*

Recommendations - Renovation

enforcement of no parking policy and one-way traffic.

Commercial

Commercial is one of the few east-west oriented alleys in Chinatown. Very visible, it has a potential to visually link with the Commercial Street east of Kearny, providing for a more direct connection into Chinatown from the Financial District.



Key Issues

- Commercial Alley is the only Chinatown alley left with brick paving that dates back to the early 1900's. However, over time, it has been repaired haphazardly with asphalt patches, leaving the roadway bumpy and rough.
- Double parked cars sometimes impede traffic movement in this alley, because it is only wide enough to accommodate one lane of through traffic and one lane of on-street parking.
- Without proper enforcement, metered on-street parking turn-over rate is low.
- Parking and loading zones are deemed highly desirable in this alley due to the presence of a number of large commercial establishments.
- Existing lights are city standard cobra lights (4 total); street lighting has been observed by alley users to be inadequate.

Physical Improvement Proposals for Commercial Paving

- Concrete paving. Reconstruct the roadway. Retain existing brick for paving bands at key points in the alley (see page 37.) Continue similar paving colors as Commercial Street below.
- Pedestrian bulbs. Install small pedestrian bulbs adjacent to the north sidewalk and place planters there for shrubs/small trees.

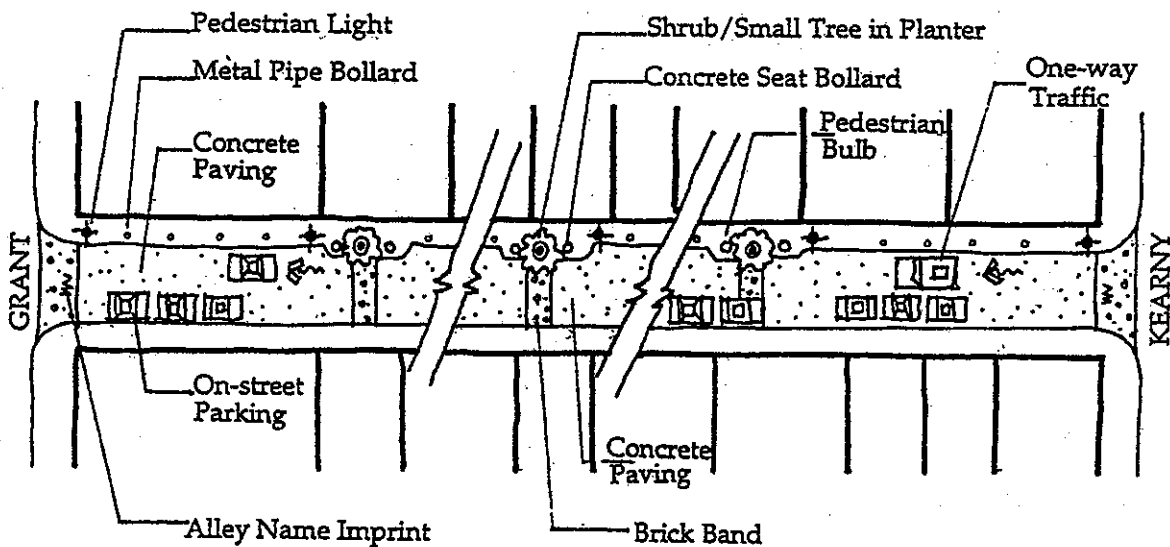
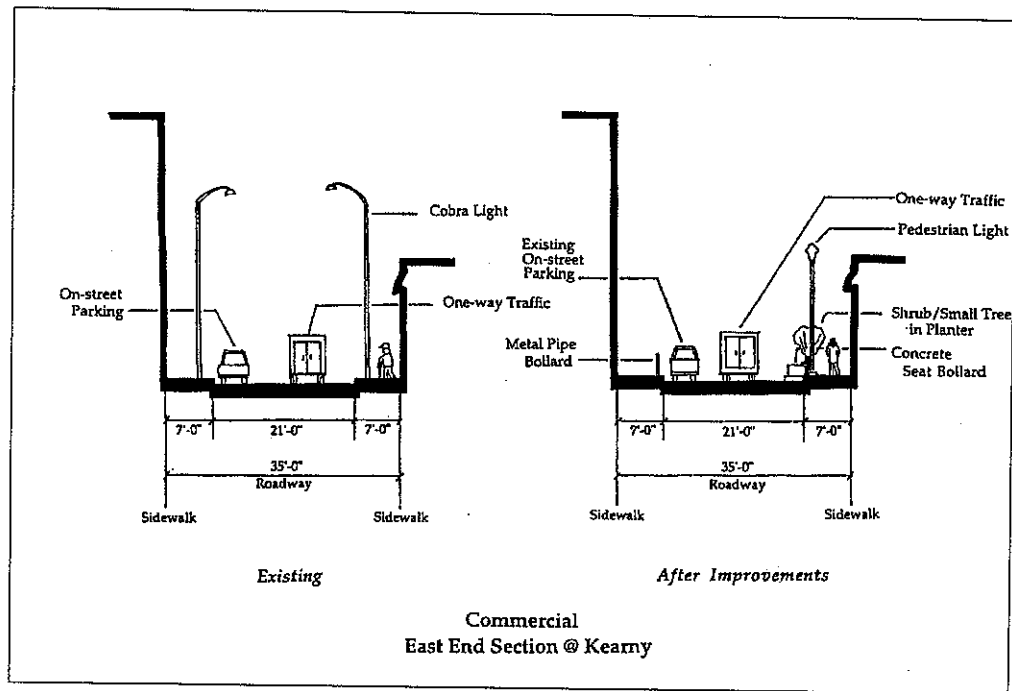
Lighting

- City standard pedestrian lights. Remove existing city standard cobra lights and replace with pedestrian scale lights (see page 99 - Lights.)

Landscaping

- Shrubs/small trees in planters.

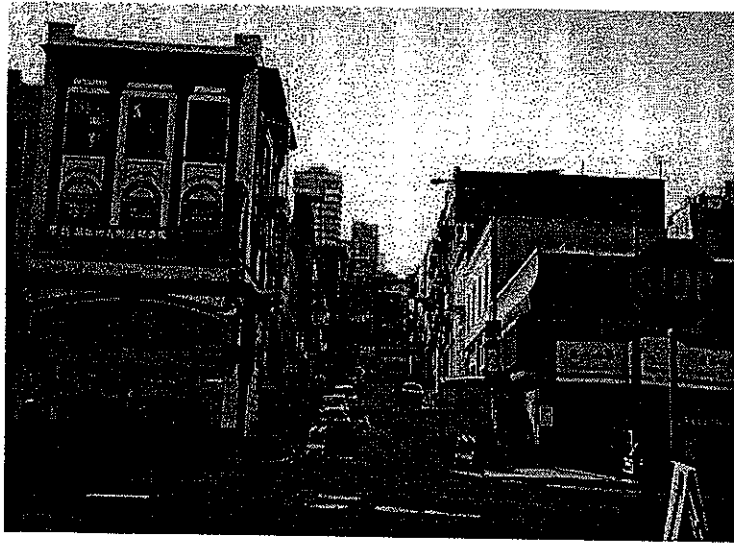
Recommendations - Renovation



Commercial

0 10 20 40 60 Ft

Recommendations - Renovation



Commercial Alley as Existing: Looking West from Kearny

Traffic

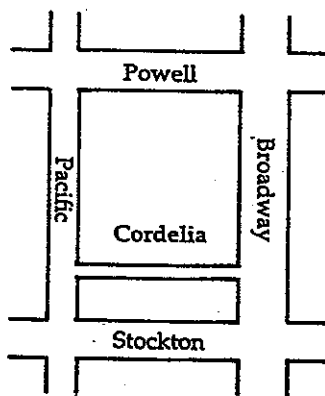
- Sidewalks and curbs. Reconstruct sidewalks and curbs.
- One-way street. Allow vehicular movement one-way from Kearny to Grant as is.
- Bollards. Locate metal pipe bollards on the north sidewalk to clearly separate pedestrian zones from the roadway.
- Retain existing on-street metered parking as is.

Other Enhancements

- Alleyway name in English and Chinese imprinted in paving (see page 31 - General Plan.)
- Special Art Projects: A wall mounted plaque acknowledging the alley's historic importance and decorative planters (see pages 31 to 32 - General Plan.)

Policy Visions for Commercial

- Garbage enforcement. Ensure regular enforcement of garbage and unsanitary conditions in the alley.
- Street cleaning and sweeping. Ensure regular cleaning and sweeping of the alley.
- Parking and one-way street enforcement. Ensure regular enforcement of metered parking and one-way traffic.



Cordelia

Located next to Broadway at the edge of the Chinatown core area, Cordelia is used primarily as a passageway; it is a convenient short-cut for both pedestrians and cars.

Key Issues

- Garbage containment and litter control are major issues in the alley. Commercial garbage dumpsters (cubic yard cans on wheels) in the alley, adjacent to the businesses on the east side, are always open and accessible. They attract outsiders to come

Recommendations - Renovation

into the alley and illegally dump garbage.

- Residents complain that garbage collection times in the alley are too early (4 to 5 a.m.) They are awakened almost daily by garbage trucks.
- Cars, especially on the weekends, illegally park on the west sidewalk adjacent to the Bayside Senior Center, taking away pedestrian space. Improvements are needed to better direct traffic and enforce parking activities.
- The alley is currently lit by a city standard cobra light (1 total) and three private wall mounted lights (on the side of the Bayside Senior Center); existing lights are observed by residents to be inadequate.



Commercial Garbage Dumpsters at Cordelia Alley

Physical Improvement Proposals for Cordelia

Paving

- Concrete paving. Reconstruct the roadway.

Lighting

- City standard pedestrian lights. Remove existing city standard cobra lights and replace with pedestrian scale lights (see page 99 - Lights.)

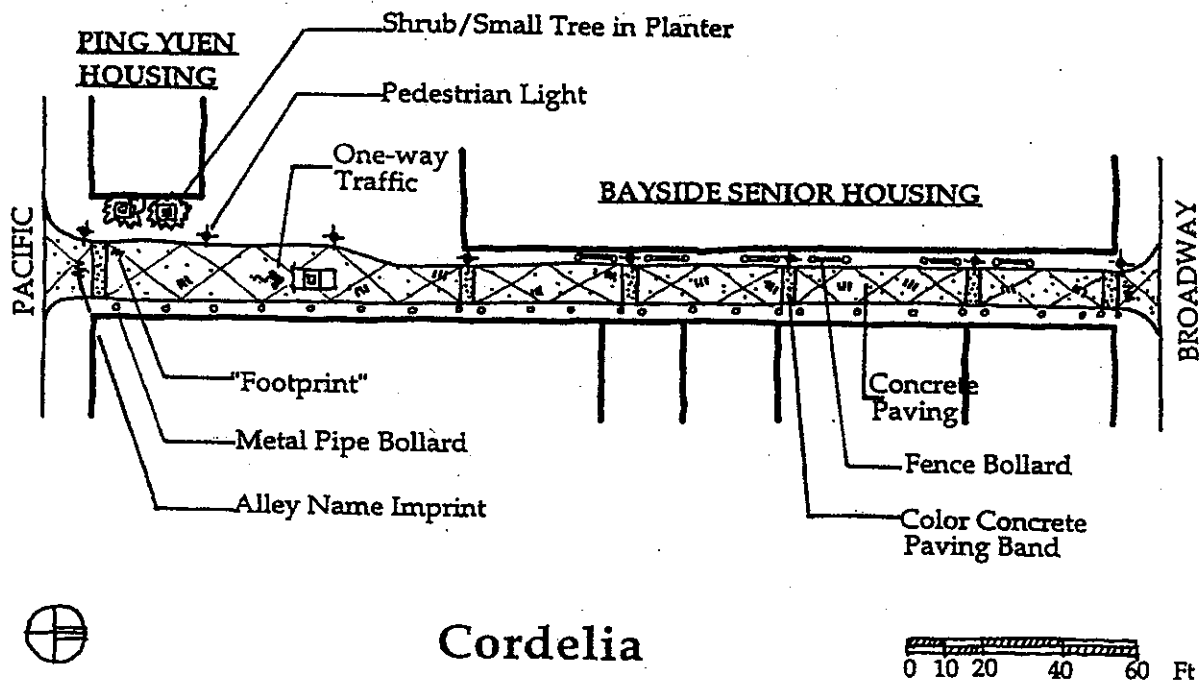
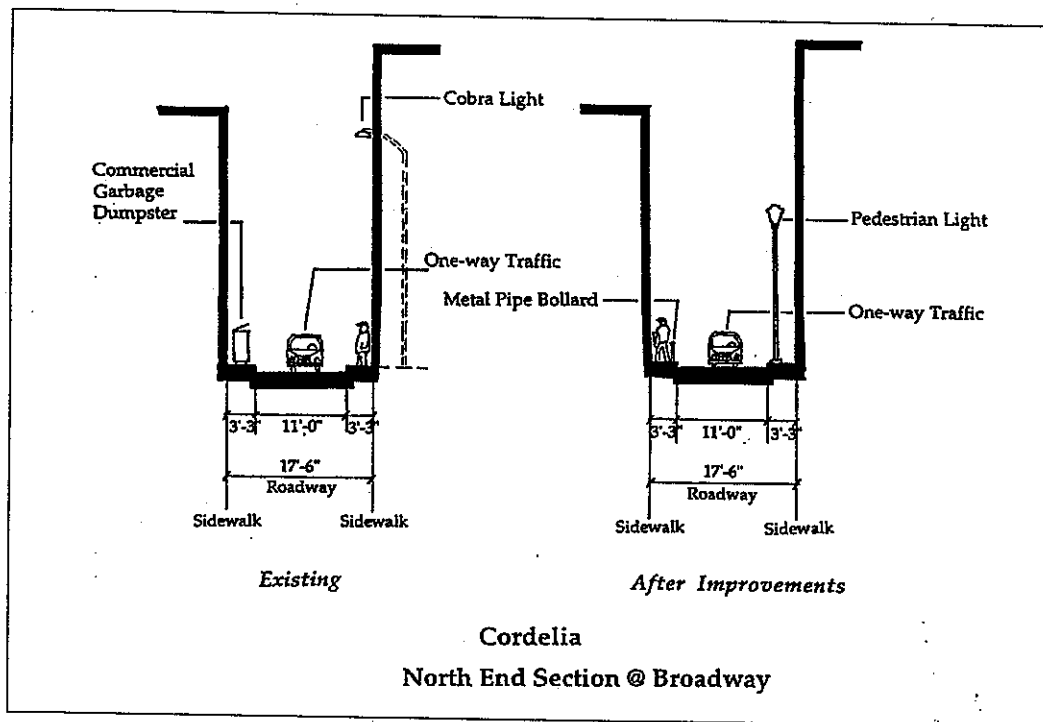
Landscaping

- Shrubs/small trees in planters at the south entrance to the alley.

Traffic

- Sidewalks and curbs. Reconstruct sidewalks and curbs.
- One-way street. Allow vehicular movement one-way from Pacific to Broadway as is.
- Bollards. Locate two types of bollards, metal pipe on the east sidewalk and fence type on the west sidewalk, to clearly separate pedestrian zones from the roadway.
- Allow no on-street parking as is.

Recommendations - Renovation



Recommendations - Renovation

Other Enhancements

- Alleyway name in English and Chinese imprinted in paving (see page 31 - General Plan.)
- Special Project: Footprints across Chinatown (see page 32 - General Plan.)

Policy Visions for Cordelia

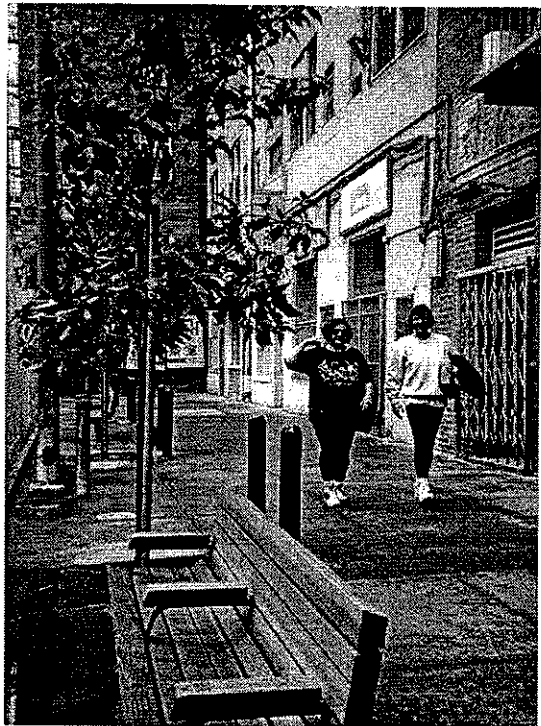
- Remove all on-site commercial garbage dumpsters. Businesses, as required by law, should contain all garbage bins on their own premises (see pages 84 to 85 - Maintenance Plan.)
- Garbage enforcement. Ensure regular enforcement of garbage dumping and unsanitary conditions in the alley.
- Street cleaning and sweeping. Ensure regular cleaning and sweeping of the alley.
- Parking and one-way street enforcement. Ensure regular enforcement of illegal parking and one-way traffic.
- Evaluate garbage collection times. Evaluate existing garbage collection times and recommend changes as appropriate.

Hang Ah or Pagoda

Note: Funded by the Community Development Block Grant, Hang Ah Alley underwent renovation work in October, 1996. It is the first alley in the Alleyway Master Plan to be renovated.

The following improvements were completed for Hang Ah Alley. (Those listed as "future project" are not completed at this time and will be pursued in the Alleyway Master Plan.)

- The entire street was reconstructed with a careful selection of materials and colors, some of which will set a precedent for future alleyway projects.
- Removed existing curb return at Sacramento and provided a one grade sidewalk at the entrance to the alley, making the alley more accessible.
- Taking advantage of the alley's open quality, trees were installed. In addition to the trees, a special vine



Hang Ah Alley after the Jan., 1997 Renovation

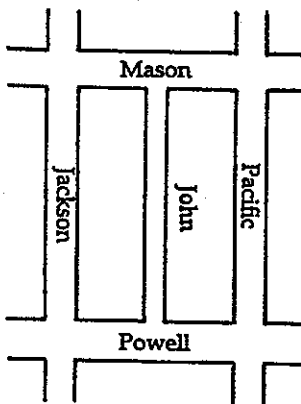
Recommendations - Renovation



New Metal Vine Trellis at Hang Ah Alley

trellis was placed at the Clay entrance to the alley.

- Benches and concrete bollards that could be interchangeably used as seats and tables were installed as a way to encourage recreational uses associated with the alley.
- Removed all on-site public garbage cans; the cans attracted more illegal dumping than did they benefit the alley.
- Special Art Project: Alleyway name in English and Chinese was imprinted in paving.
- Special Art Project: Hang Ah alleyway history in English and Chinese imprinted in paving bands at both ends of and throughout the alley.
- **Future Project:** Wall mural at the entrance to the alley on the Clay Street side.
- **Future Project:** Evaluate and if needed, upgrade existing city standard cobra lights (3 total.)
- **Future Project:** Establish a street closure. Between 10 to 4, the alley is open for pedestrians only (see page 110 - Access.) Allow delivery trucks' access before 10 a.m. and after 4 p.m. only.
- **Future Project:** Install "Footprints across Chinatown" (see page 32 - General Plan.)



John

Primarily adjacent to residential uses and away from the commercial core of Chinatown, John Alley enjoys peace and quiet atypical of most other Chinatown alleyways. The future for John includes a new park for Chinatown, to be located at the corner of John and Powell Street. Historically, John has had problems with vehicles entering the narrow lane from both Mason and Powell. At the request of residents, a one-way traffic direction (from Powell to Mason) was established in 1996.

Key Issues

- On-street residential parking is not enforced; many people from outside the area, without residential permits, have been observed to park in the alley all day.
- Existing lights are PG&E wood pole lights (4 total) serviced by

Recommendations - Renovation

overhead wires. It is observed by residents to be inadequate.

Physical Improvement

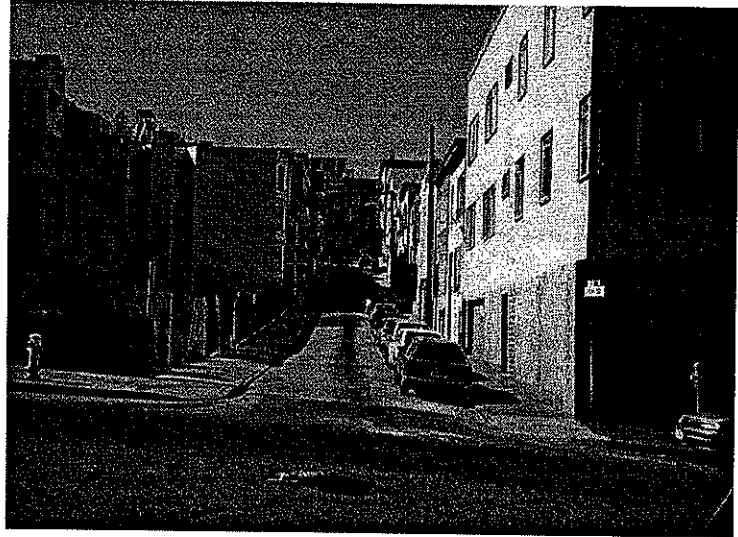
Proposals for John

Paving

- Concrete paving. Reconstruct the roadway.

Lighting

- City standard pedestrian lights. Remove existing PG&E lights, underground overhead wires and install new pedestrian scale lights (see page 99 - Lights.)



John Alley as Existing: Looking West from Powell

Landscaping

- Street trees. Add street trees to the south sidewalk adjacent to the park.

Traffic

- Sidewalks and curbs. Reconstruct sidewalks and curbs.
- One-way street. Allow vehicular movement one-way from Mason to Powell as is.
- Bollards. Locate metal pipe bollards on the south sidewalk to clearly separate pedestrian zones from the roadway.
- Retain existing residential permit parking as is.

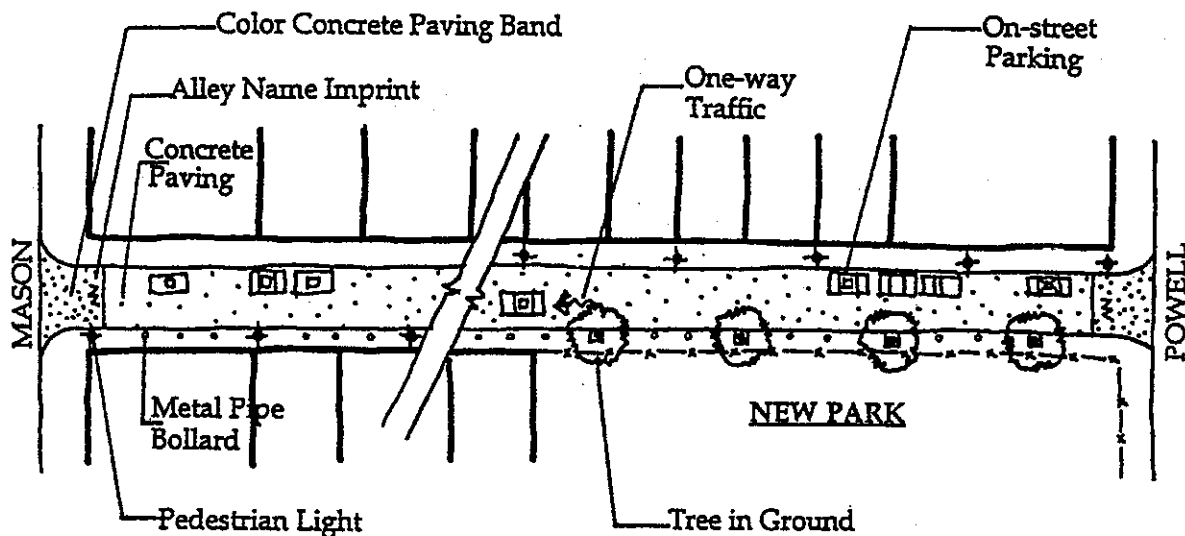
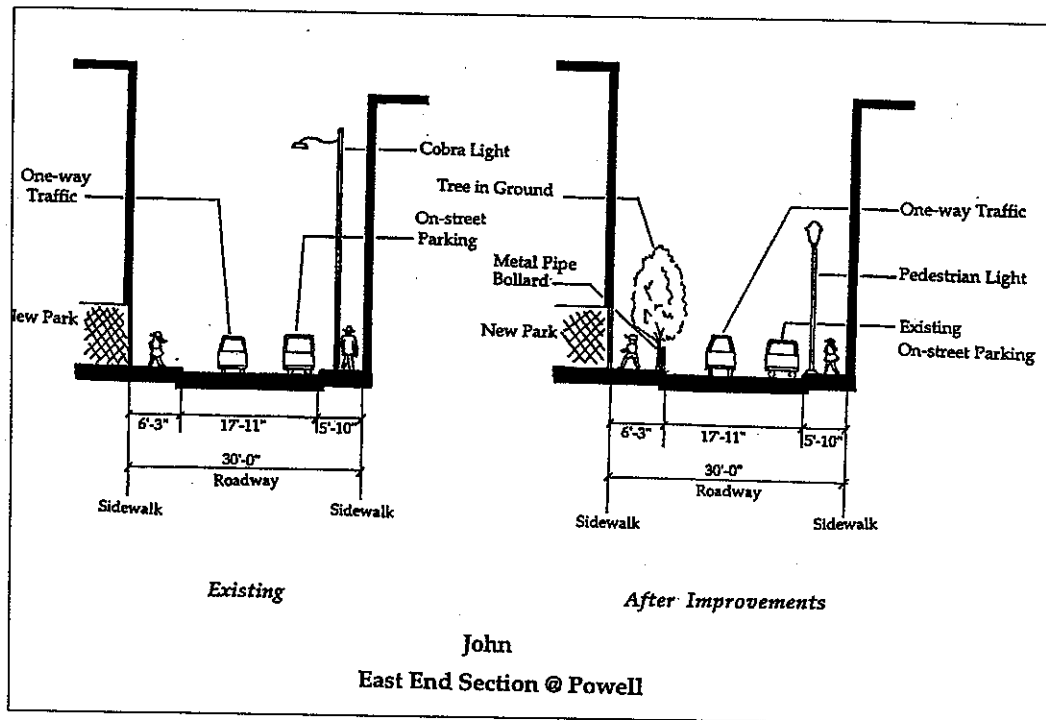
Other Enhancements

- Alleyway name in English and Chinese imprinted in paving (see page 31 - General Plan.)

Policy Visions for John

- Street cleaning and sweeping. Ensure regular cleaning and sweeping of the alley.
- Parking and one-way street enforcement. Ensure regular enforcement of residential permit parking and one-way traffic.

Recommendations - Renovation



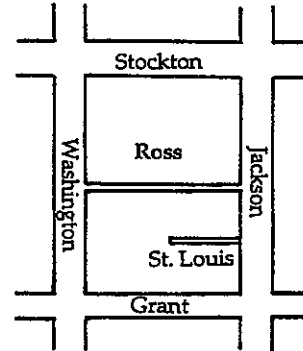
John

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Recommendations - Renovation

Ross

As one of the first to be established, Ross Alley is imbued with Chinatown's history. Once known as the Old Spanish Alley, it was in the early 1900's a destination point for men of Latin descent who frequented the gambling and prostitution houses located there. Today, it is a busy alley with many neighborhood businesses, residences and family associations.



Key Issues

- Although a dedicated alley, Ross is currently not "accepted" by the City for maintenance. It is an objective of the *Alleyway Master Plan* to see that it is accepted for maintenance after renovation.
- Existing paving, last renovated in 1981, is old. Surface drainage conditions are unreliable; sometimes, water sits in the alley.
- The alley is too narrow and often, when cars are in the alley, pedestrian movement is very difficult and unsafe. Vehicular movement should be better contained and enforced.
- A cleaner alley would be a major improvement for this alley. As it is, the alley is very dirty; it suffers from illegal garbage dumping and littering.
- Existing lights are city standard cobra lights poles (2 total); the lights are poor and inadequate for many users of this alley.

Physical Improvement Proposals for Ross

Paving

- Concrete paving. Reconstruct the entire one-level alley.

Lighting

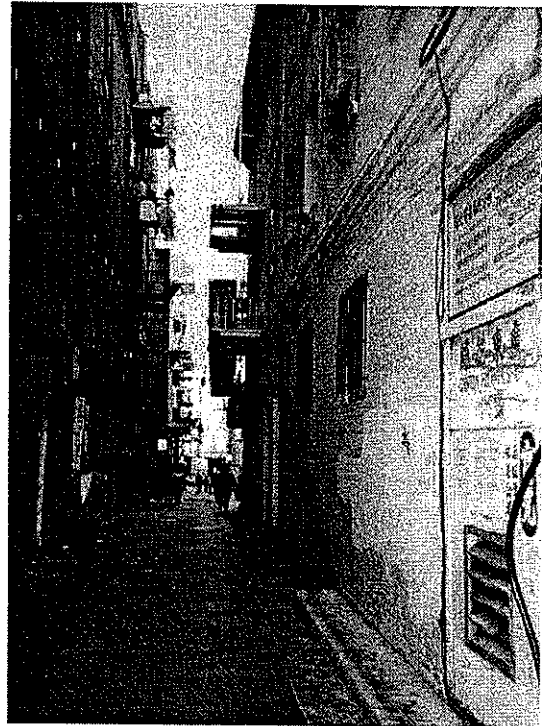
- City standard pedestrian lights. Remove existing city standard cobra lights and replace with pedestrian scale lights (see page 99 - Lights.)

Landscaping

- Shrubs in planters.

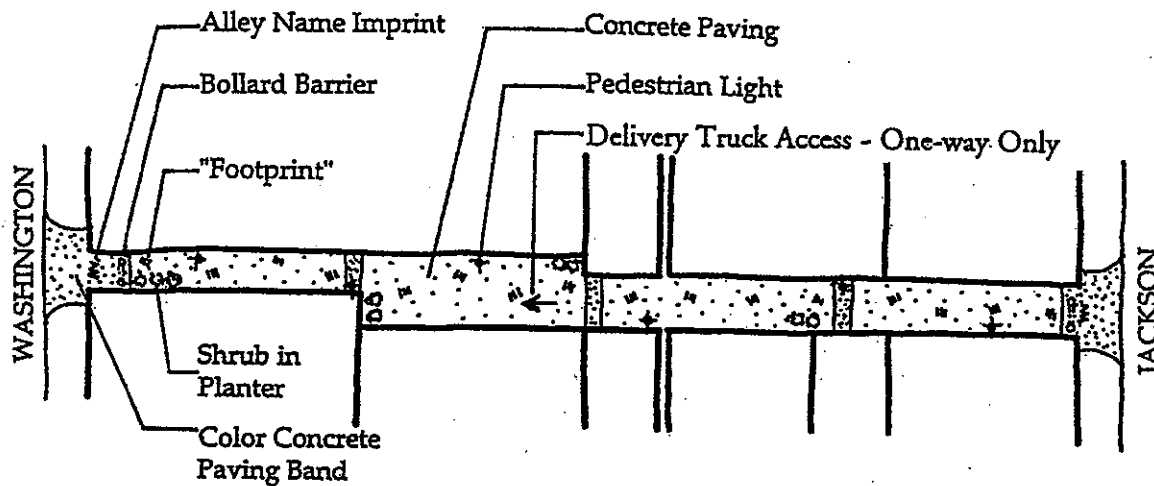
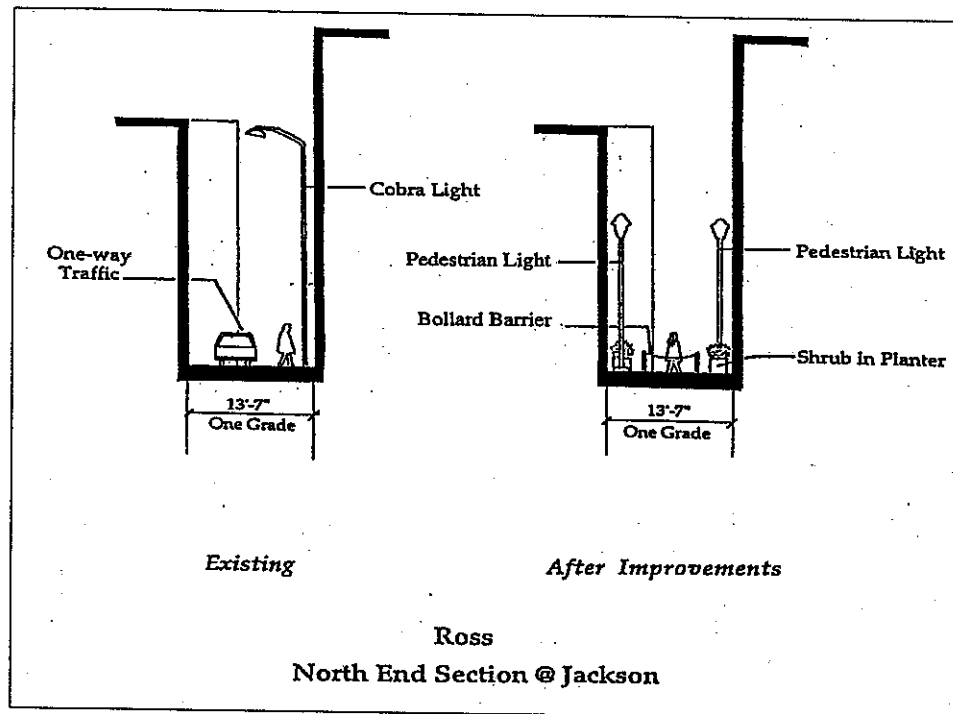
Traffic

- Establish a street closure. As a three month trial run,



*Ross Alley as Existing:
Looking North from Washington*

Recommendations - Renovation



Ross

0 10 20 40 60 Ft

Recommendations - Renovation

put in place a street closure policy. Between 10 a.m. to 4 p.m., the alley is open for pedestrians only. Allow delivery trucks access, one way from Jackson to Washington, before 10 and after 4 only. At the end of the three month period, get additional community input to consider the policy permanently (see page 110 - Access.)

- One grade alley. The alley remains at one-grade as is.
- Bollards. Use bollard barriers to close the alley to cars.
- Allow no on-street parking as is.

Other Enhancements

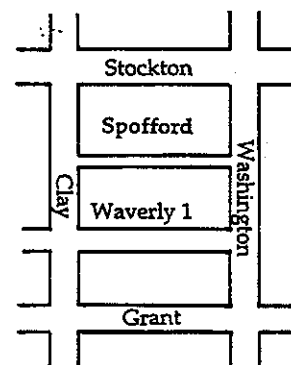
- Alleyway name in English and Chinese imprinted in paving (see page 31 - General Plan.)
- Special Project: Footprints across Chinatown (see page 32 - General Plan.)
- Special Art Projects: a wall mounted plaque acknowledging the alley's historic importance, decorative planters and ground imprints (see pages 31 to 32 - General Plan.)

Policy Visions for Ross

- Ensure the alley's "acceptance" for maintenance (see page 103.)
- Remove all on-site public garbage cans; they attract illegal dumping more so than help the alley environment.
- Garbage enforcement. Ensure regular enforcement of illegal garbage dumping and unsanitary conditions in the alley.
- Street cleaning and sweeping. Ensure regular cleaning and sweeping of the alley.
- Limited vehicular access enforcement. Ensure regular enforcement of limited traffic access to the alley.
- Parking enforcement. Ensure regular enforcement of a no parking policy in the alley.

Spofford

Like Ross Alley, Spofford is one of the first established alleyways in Chinatown and is historically significant. Many community oriented uses are still located there, the most prominent of which are the mah jong parlors, where families and friends meet to play a very popular Chinese board game using tiles.



Recommendations - Renovation

Key Issues

- Last renovated in 1983, paving is old; many areas have pot holes.
- Sunny yet well protected from the outside streets, Spofford Alley is ideal for recreational use. However, such a use is found to be unsafe, because there is much conflict with automobile and pedestrian uses.
- The alley is very dirty; it suffers from illegal garbage dumping and littering.
- Existing lights are PG&E wood pole lights (3 total) serviced by overhead wires. It is observed by residents to be inadequate.

Physical Improvement Proposals for Spofford

Paving

- Concrete paving. Reconstruct the entire one-level alley.

Lighting

- City standard pedestrian lights. Remove existing PG&E lights, underground overhead wires and install new pedestrian scale lights (see page 99 - Lights.)

Landscaping

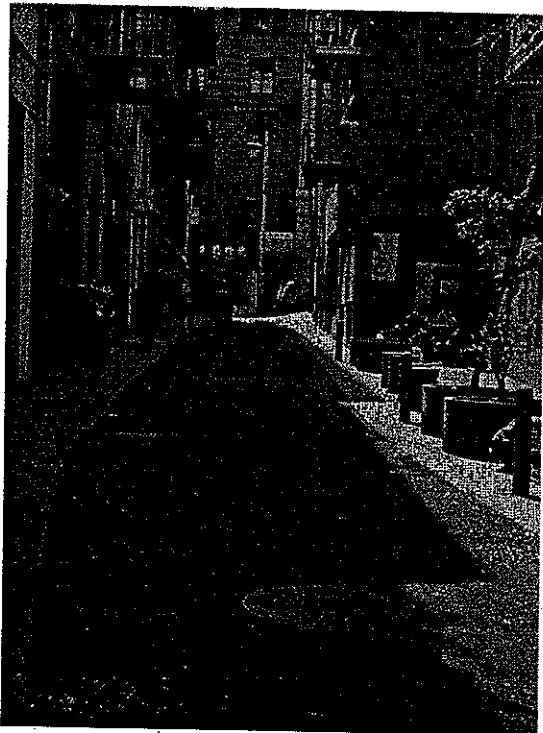
- Shrubs in planters.

Traffic

- Establish a street closure. As a three month demonstration project, put in place a street closure policy. Between 10 a.m. and 4 p.m., the alley is open for pedestrian use only. Allow delivery trucks access, one-way from Clay to Washington, before 10 and after 4 only. At the end of the three month trial period, get additional community input to consider the policy permanently (see page 110 - Access.)
- One grade alley. The alley remains at one-grade as is.
- Bollards. Use bollard barriers to close the alley to vehicular traffic.
- Allow no on-street parking as is.

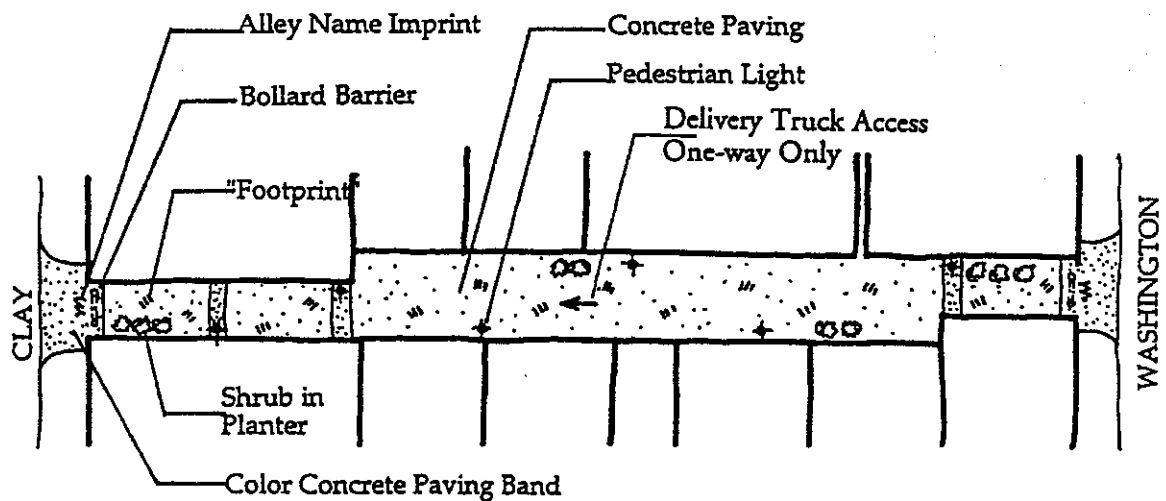
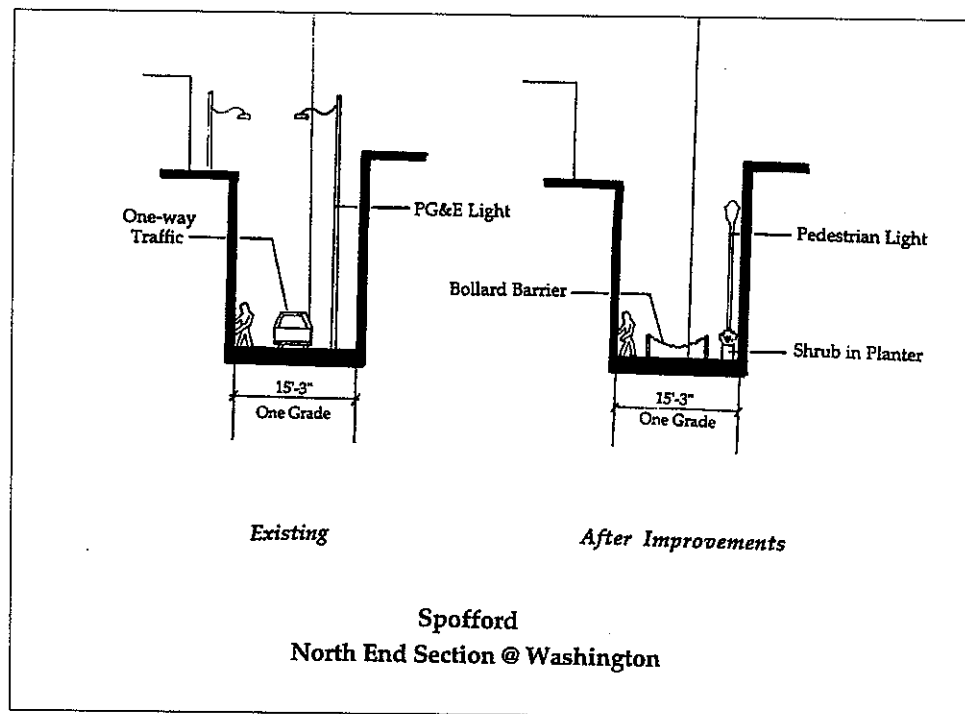
Other Enhancements

- Alleyway name in English and Chinese imprinted in paving (see page 31 - General Plan.)



*Spofford Alley as Existing:
Looking South from Washington*

Recommendations - Renovation



Spofford

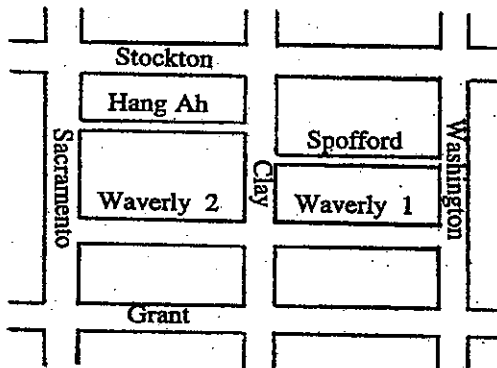
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Recommendations - Renovation

- Special Project: Footprints across Chinatown (see page 32 - General Plan.)
- Special Art Projects: a wall mounted plaque acknowledging the alley's historic importance, decorative planters and ground imprints (see pages 31 to 32 - General Plan.)

Policy Visions for Spofford

- Remove all on-site public garbage cans; they attract illegal dumping.
- Garbage enforcement. Ensure regular enforcement of illegal garbage dumping and unsanitary conditions in the alley.
- Street cleaning and sweeping. Ensure regular cleaning and sweeping of the alley.
- Limited vehicular access enforcement. Ensure regular enforcement of limited traffic access to the alley.
- Parking enforcement. Ensure regular enforcement of a no parking policy in the alley.



Waverly 1 & Waverly 2

Waverly 1 and 2 are alleys unquestionably associated with Chinatown's tradition and past; many community oriented uses such as temples and family associations are still located there. Both alleys are very well recognized; they are one of the most visited and photographed alleys. Large sidewalks, varied building facades and ground floor commercial interest are characteristic of the two blocks.

Key Issues

- A cleaner environment with proper places to contain trash would enhance the alley; education to the businesses in the area is needed.
- Without enforcement, double parking in the alley is common. Many cars and delivery trucks illegally park on the west sidewalk.
- Existing lights are PG&E wood pole lights (2 total each alleyway) serviced by overhead wires. Current lighting is observed by residents to be inadequate.

Recommendations - Renovation

Physical Improvement Proposals for Waverly 1 & Waverly 2

Paving

- Concrete paving. Reconstruct the roadway. To better connect the two blocks of Waverly, continue the new paving to the pedestrian crosswalks at Clay (see pages 52 to 53.)

Lighting

- City standard pedestrian lights. Remove existing PG&E lights, underground overhead wires and install new pedestrian scale lights (see page 99 - Lights.)

Landscaping

- Shrubs/small trees in planters. Locate planters on the west sidewalk.

Traffic

- Sidewalks and curbs. Reconstruct sidewalks and curbs. Widen the west sidewalk by at least two feet. The east sidewalk will remain as is; existing on-street parking will not be affected.
- One-way street. Allow vehicular movement one-way from Washington to Clay for Waverly 1 and from Clay to Sacramento for Waverly 2 as is.
- Bollards. Locate metal pipe bollards on the west sidewalk to clearly separate pedestrian zones from the roadway. Also install concrete seat bollards on the west sidewalk.
- Retain existing residential permit parking as is.

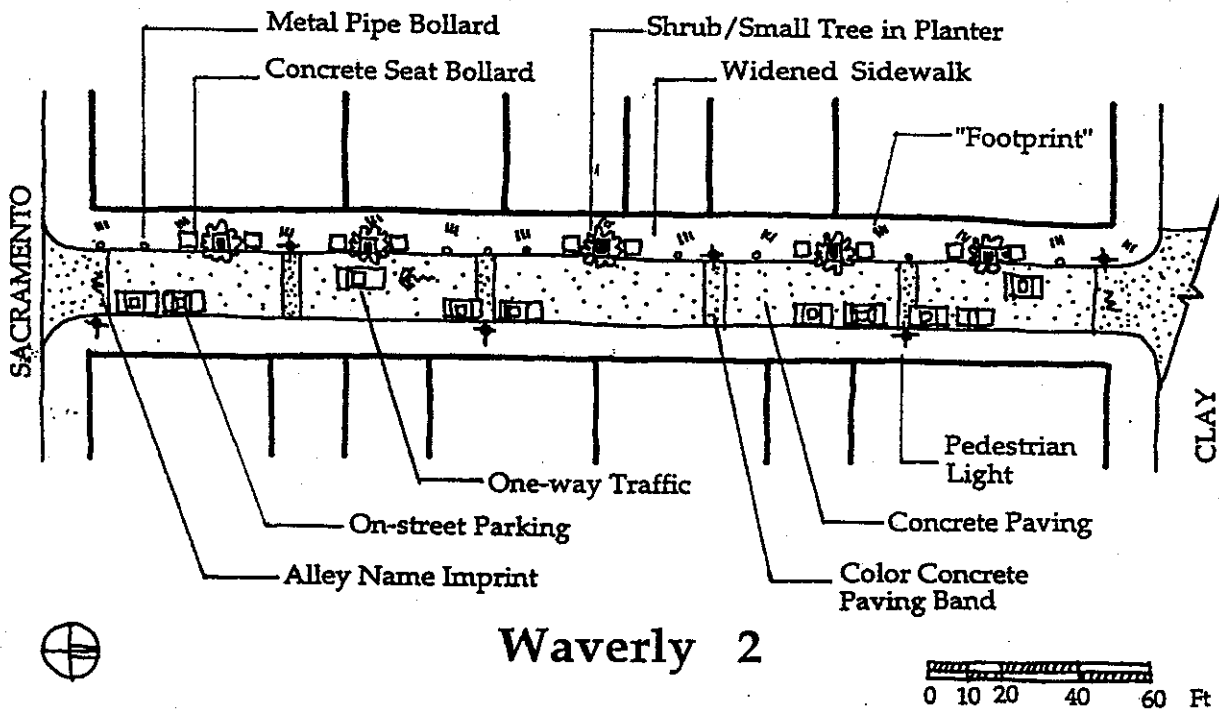
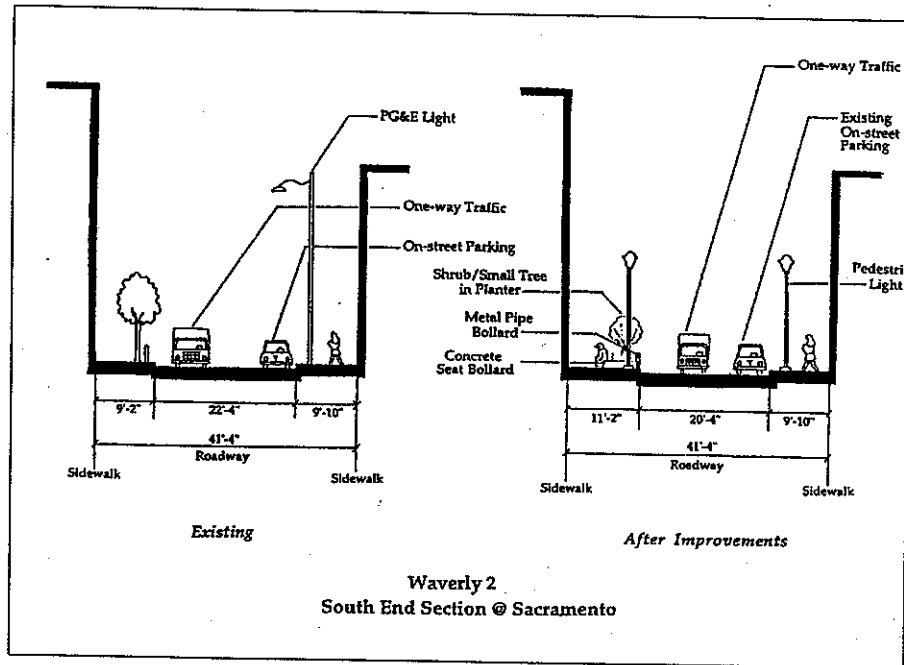
Other Enhancements

- Alleyway name in English and Chinese imprinted in the paving (see page 31 - General Plan.)
- Special Project: Footprints across Chinatown (see page 32 - General Plan.)
- Special Art Projects: A wall mounted plaque acknowledging the alley's historic importance and decorative planters (see pages 31 to 32 - General Plan.)

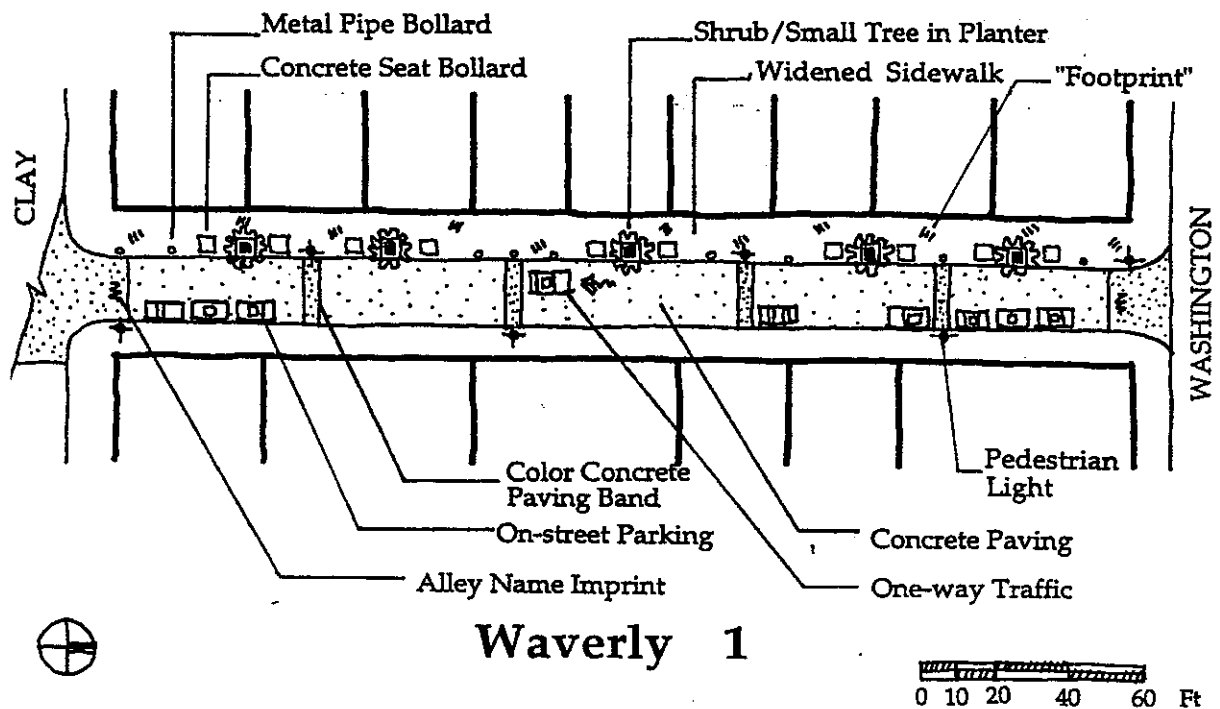
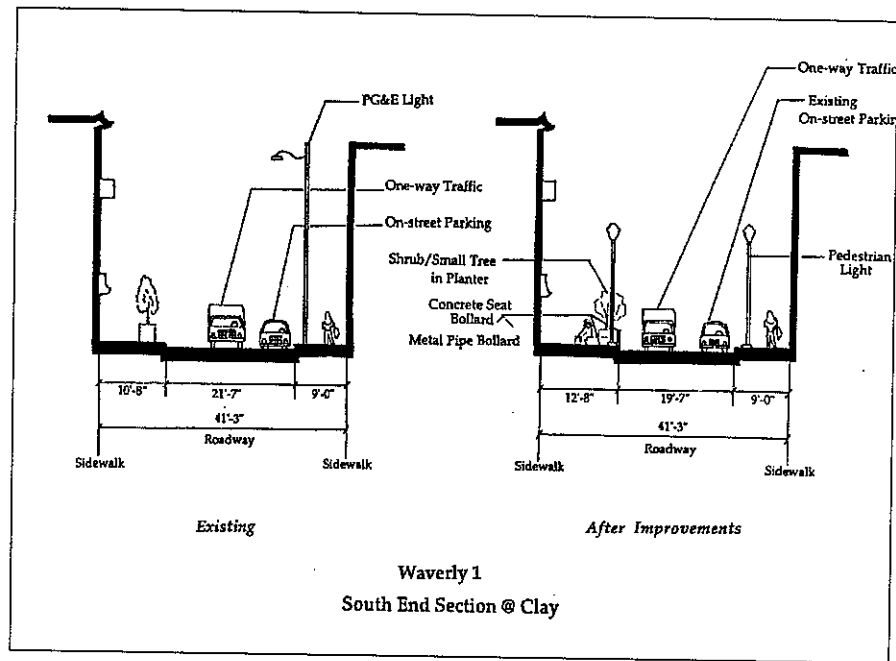


Waverly 1 as Existing: Looking North from Clay

Recommendations - Renovation



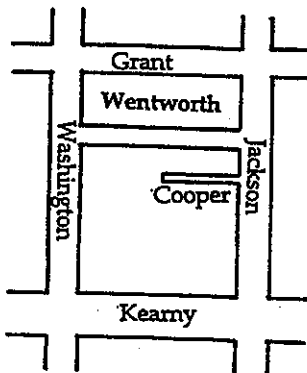
Recommendations - Renovation



Recommendations - Renovation

Policy Visions for Waverly 1 & Waverly 2

- Remove all on-site public garbage cans; they attract illegal dumping.
- Remove all on-site commercial garbage dumpsters. Businesses, as required by law, should contain all garbage bins on their premises (see pages 84 to 85 - Maintenance Plan.)
- Street cleaning and sweeping. Ensure regular cleaning and sweeping of the alley.
- Parking and one-way street enforcement. Ensure regular enforcement of residential permit parking and one-way traffic in the alley.



Wentworth

Wentworth is a historically significant alley with interesting building facades and with many ground floor commercial activities. Like Beckett, Wentworth has a good potential to be a pedestrian way.

Key Issues

- Also like Beckett, although wider, Wentworth is too narrow to safely accommodate both on-street truck parking and through traffic. Pedestrians walk in the street because sidewalks are too narrow.
- On-street truck parking has a low turn-over. Because the alley is narrow, on-street parking is more a deterrent for an efficient traffic flow.
- With parked trucks as a barrier, the west sidewalk area adjacent to the parking is a favorite place for unsavory activities. Robberies, urination, and illegal garbage dumping are major complaints of the alleyway users.
- Existing lights are PG&E wood pole lights (2 total) serviced by overhead wires. It is observed by residents to be inadequate.

Physical Improvement Proposals for Wentworth

Paving

- Concrete paving. Reconstruct the roadway.

Lighting

- City standard pedestrian lights. Remove existing PG&E lights,

Recommendations - Renovation

underground overhead wires and install new pedestrian scale lights.

Landscaping

- Shrubs/small trees in planters. Add street trees/shrubs in planters at key points to mark and enhance the alley (see page 56.)

Traffic

- Sidewalks and curbs. Reconstruct sidewalks and curbs.
- One-way street. Allow vehicular movement one-way from Jackson to Washington as is.
- Bollards. Locate metal pipe bollards on the east sidewalk to clearly separate pedestrian zones from the roadway.
- Implement a green zone for short-term parking and for delivery needs. In place of existing truck parking, allow for short-term parking (for loading and unloading purposes only) with a delineation of a green zone on the west sidewalk.



Wentworth Alley as Existing: Looking South from Jackson

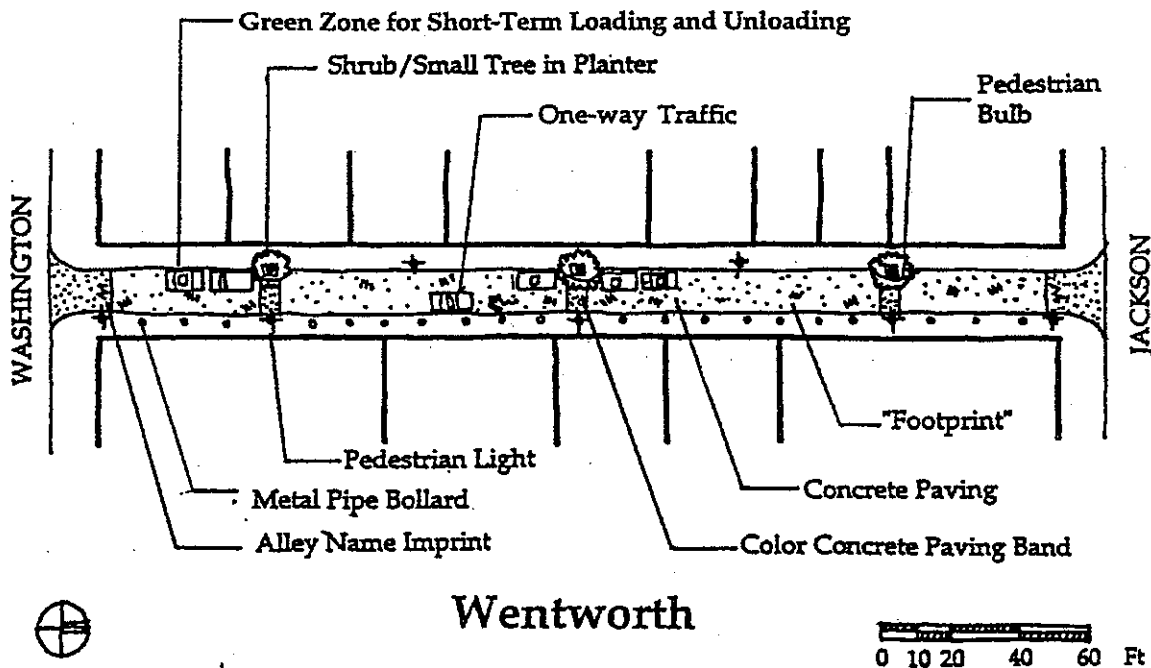
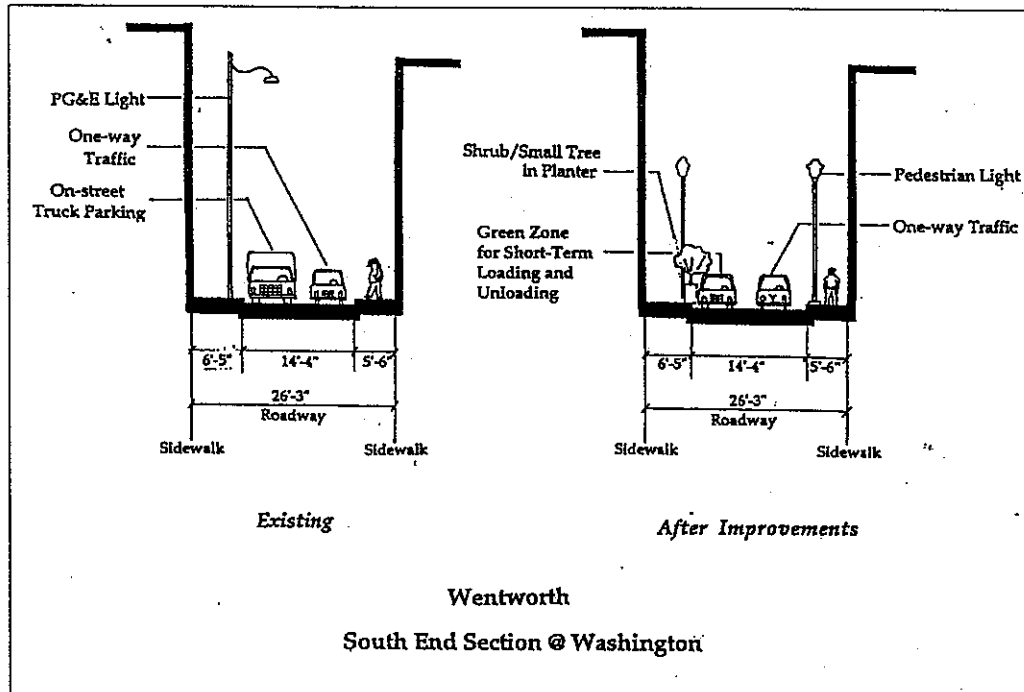
Other Enhancements

- Alleyway name in English and Chinese imprinted in paving (see page 31 - General Plan.)
- Special Project: Footprints across Chinatown (see page 32 - General Plan.)
- Special Art Projects: decorative planters (see page 32 - General Plan.)

Policy Visions for Wentworth

- Street cleaning and sweeping. Ensure regular cleaning and sweeping of the alley.
- Garbage enforcement. Ensure regular enforcement of illegal garbage dumping and unsanitary conditions in the alley.
- Parking and one-way street enforcement. Ensure regular enforcement of the green zone and one-way traffic in the alley. No long-term parking is allowed.

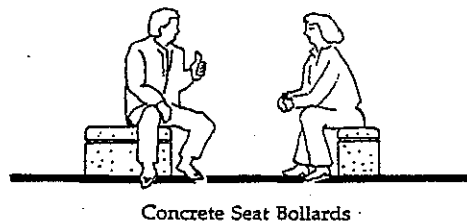
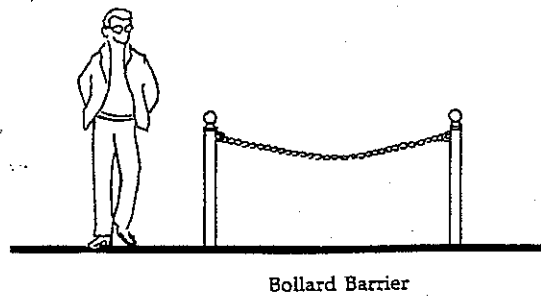
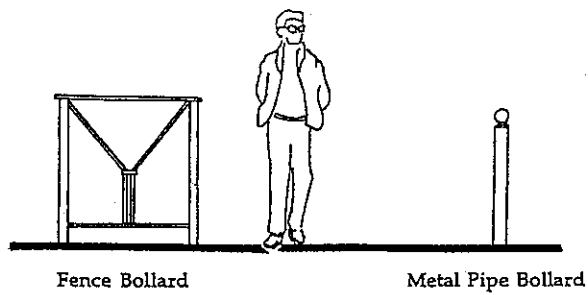
Recommendations - Renovation



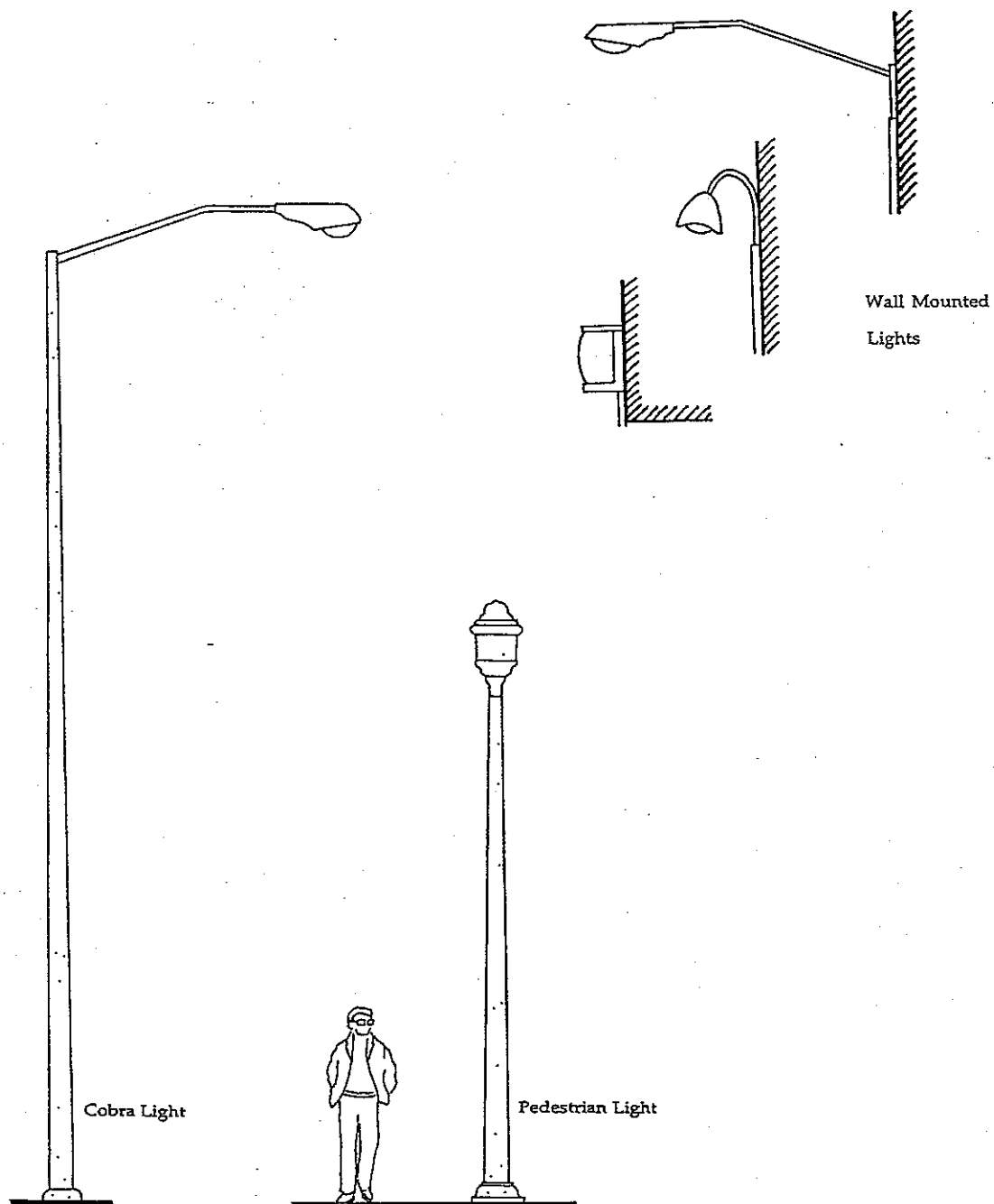
Recommendations - Renovation

Conceptual Sketches of Details Referenced in the Phase I Alley Renovation Plans

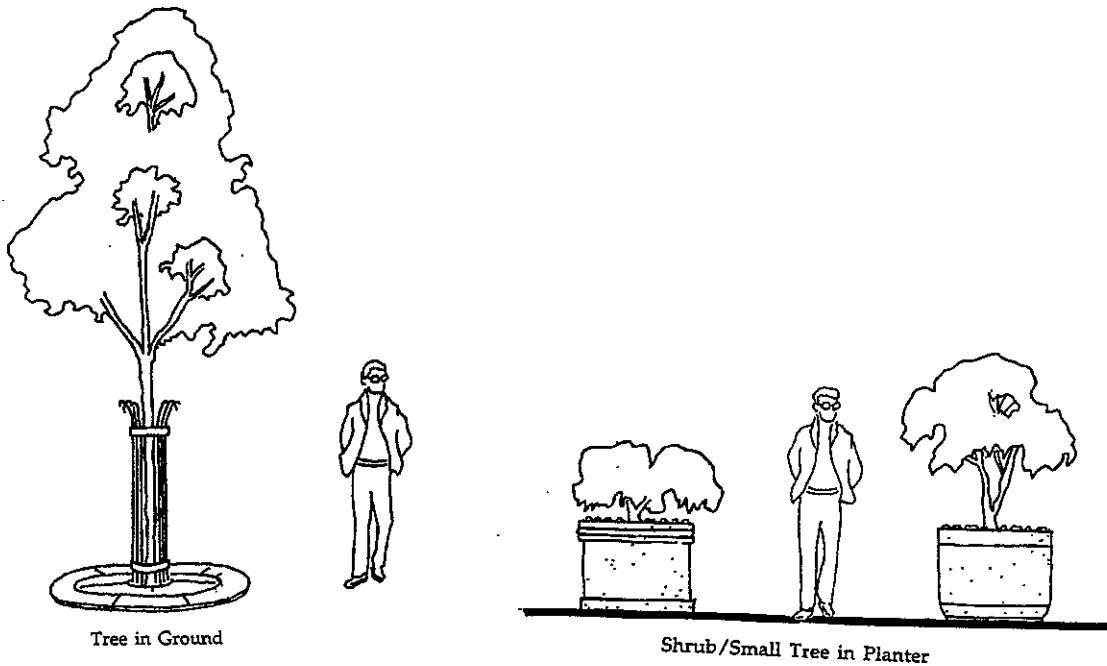
*Note: The following details are also applicable to Phase II and
Phase III Alleyways.*



Recommendations - Renovation



Recommendations - Renovation



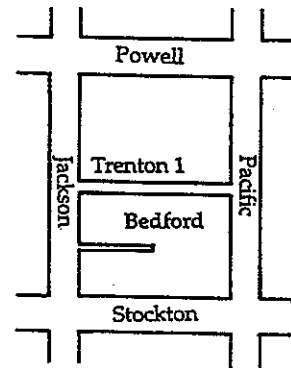
Phase II Alleys

Bedford

Bedford, primarily a back alley, is commonly utilized by nearby businesses as a convenient staging area.

Key Issues

- The alley is used mainly for walk-in deliveries and as a loading and unloading area for businesses; it is often cluttered with cardboard boxes and is dirty.
- The steepness of the alley makes it difficult for pedestrians to access the alley.
- This alley currently has no lights, private nor public.



Physical Improvement Proposals for Bedford

Paving

- Asphalt-concrete paving. Resurface the entire alley.

Recommendations - Renovation

Lighting

- City standard wall mounted lights. Install city standard wall mounted lights (see page 100 - Lights.)

Traffic

- Grade accommodation. Install concrete steps to improve pedestrian access to the alley.

Policy Visions for Bedford

- Garbage enforcement. Ensure regular enforcement of illegal garbage dumping and unsanitary conditions in the alley.

Cooper

Cooper is primarily a utilitarian lane with very little foot traffic. It is also one of the narrowest alleys with a width of only 5'-6" from property line to property line.

Key Issues

- Public health, safety and cleanliness are primary issues of concern at Cooper; there is generally a hopeless feeling expressed about the alley's conditions by those interviewed.
- Located in the alley are a number of useless bollards.
- The alley currently has no lights.

Physical Improvement Proposals for Cooper

Lighting

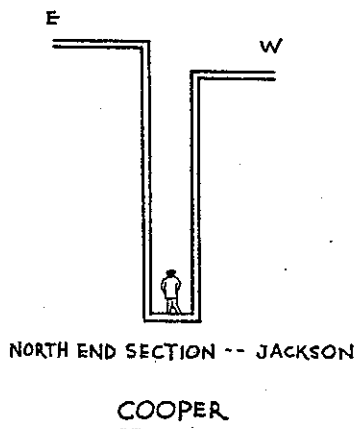
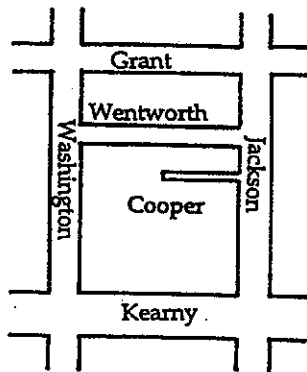
- City standard wall mounted lights. Install city standard wall mounted lights (see page 100 - Lights.)

Traffic

- One grade alley. The alley remains at one-grade as is.
- Bollards. Remove all existing bollards.

Policy Visions for Cooper

- Remove all on-site commercial garbage dumpsters. Businesses, as required by law, should contain all garbage bins on their own premises.
- Garbage enforcement. Ensure regular enforcement of illegal garbage dumping and unsanitary conditions in the alley.



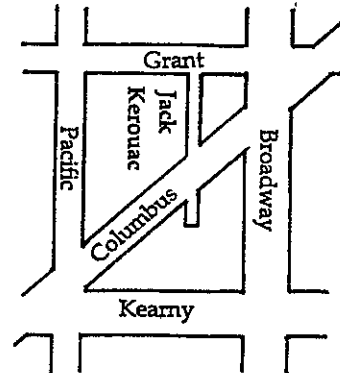
Recommendations - Renovation

Jack Kerouac or Adler

Located at the edge of the Chinatown core area, Jack Kerouac Alley functions nicely as a point of entry to Chinatown from Columbus Ave. It is a transition zone from North Beach to Chinatown and vice versa.

Key Issues

- Existing asphalt-concrete paving is old and in need of repair.
- There is only one wall mounted cobra light; it is inadequate to light the entire alley.



Physical Improvement Proposals for Jack Kerouac

Paving

- Concrete paving. Reconstruct the roadway.

Lighting

- City standard pedestrian lights. Remove the existing wall mounted light and install city standard pedestrian scale lights (see page 99 - Lights.)

Traffic

- One grade alley. Remove sidewalks and level the alley to be at one grade.
- Limited traffic access for deliveries only. Allow vehicular movement one-way from Grant to Columbus as is for deliveries only.
- Bollards. Locate metal pipe bollards in the alley to clearly separate pedestrian zones from the vehicular way.

Other Enhancements

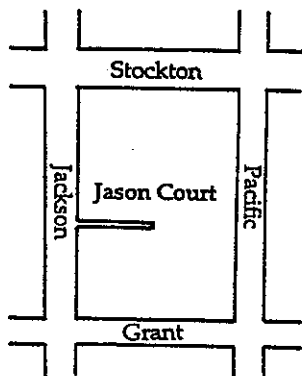
- Alleyway name in English and Chinese imprinted in paving (see page 31 - General Plan.)
- Special Art Project: a wall plaque about the alley's significance to North Beach and Chinatown, i.e. the significance of the name "Jack Kerouac" (see pages 31 to 32 - General Plan.)

Policy Visions for Jack Kerouac

- Street cleaning and sweeping. Ensure regular cleaning and sweeping of the alley.

Recommendations - Renovation

- Parking enforcement. Ensure regular enforcement of no parking policy in the alley.
- Limited vehicular access enforcement. Ensure regular enforcement of limited one-way traffic access to the alley.



Jason Court

Jason Court is a pedestrian oriented, dead-end alley. Its central location to the Chinatown core area makes it a lively space.

Key Issues

- Although a small plaza (sitting) area is in the alley, it is hardly ever used. Its presence should be re-evaluated.
- The alley suffers from bad drainage.
- The alley is currently lit by city standard cobra lights (2 total); they are observed by residents to be inadequate.

Physical Improvement Proposals for Jason Court

Paving

- Evaluate and if needed, improve existing paving and drainage.

Lighting

- City standard wall mounted lights. Remove existing city standard cobra lights and install city standard wall mounted lights (see page 100 - Lights.)

Landscaping

- Evaluate the usefulness of and if warranted, remove the plaza area.

Traffic

- One grade alley. The alley remains at one-grade as is.

Other Enhancements

- Alleyway name in English and Chinese imprinted in the paving (see page 31 - General Plan.)

Policy Visions for Jason Court

- Street cleaning and sweeping. Ensure regular cleaning and sweeping of the alley.
- Garbage enforcement. Ensure regular enforcement of illegal garbage dumping and unsanitary conditions in the alley.

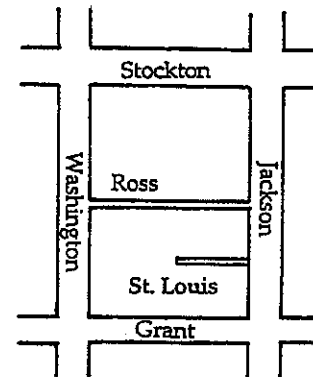
Recommendations - Renovation

St. Louis

As a quiet, dead-end alley, St. Louis is a good place for children's play. It is protected from main street activities yet close to the heart of the Chinatown core area.

Key Issues

- Evaluate the use of existing planters; they are not maintained and are sparsely planted.
- Paving could be improved and be more playful for children's use, i.e. games painted or imprinted in the ground.
- Existing lights (2 total), which are modified versions of the city standard poles (cobra) lights, are considered by residents to be inadequate.



Physical Improvement Proposals for St. Louis

Paving

- Evaluate and if needed, improve existing paving and drainage.

Lighting

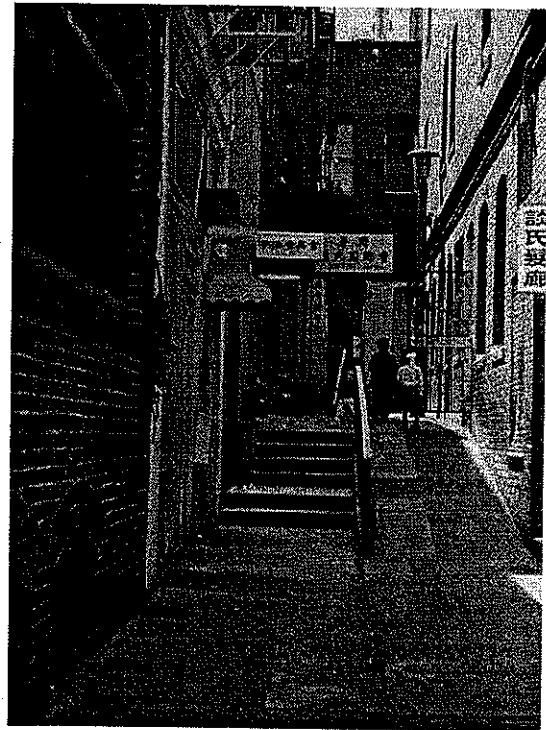
- City standard pedestrian lights. Remove existing lights and install city standard pedestrian scale lights (see page 99 - Lights.)

Landscaping

- Reconstruct two brick planters at the entrance to the alley to serve as benches.

Other Enhancements

- Alleyway name in English and Chinese imprinted in paving (see page 31 - General Plan.)
- Special Art Project: a children's game imprinted or painted in paving (see page 31 - General Plan.)

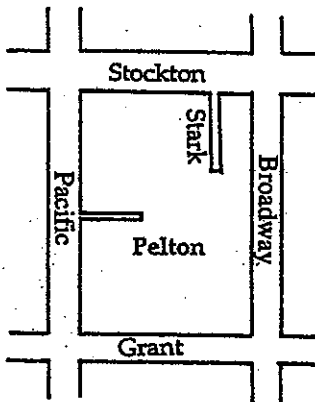


*St. Louis Alley as Existing:
Looking South from Jackson*

Policy Visions for St. Louis

- Street cleaning and sweeping. Ensure regular cleaning and sweeping of the alley.
- Garbage enforcement. Ensure regular enforcement of illegal garbage dumping and unsanitary conditions in the alley.

Recommendations - Renovation



Stark

Nothing positive was ever mentioned of Stark Alley by anyone interviewed. It functions primarily as a staging and storage area for the produce stores on Stockton Street.

Key Issues

- Existing asphalt-concrete paving is old with poor drainage.
- The only light present in the alley is a wall mounted cobra light.
- Much of the garbage created in the alley is due to the loading and unloading activities of the nearby businesses. Also, some in the community use the alley as a storage space for empty cardboard boxes.

Physical Improvement Proposals for Stark

Paving

- Asphalt-concrete paving. Resurface the roadway.

Lighting

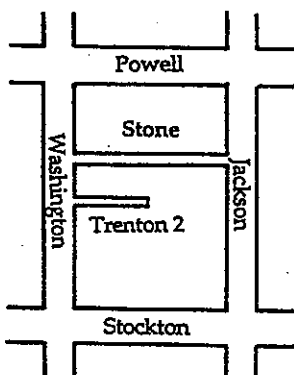
- City standard pole lights. Remove the existing wall mounted light and install city standard cobra lights (see page 100 - Lights.)

Traffic

- Sidewalk and curbs. Reconstruct sidewalks and curbs.
- Allow no on-street parking as is.

Policy Visions for Stark

- Street cleaning and sweeping. Ensure regular cleaning and sweeping of the alley.
- Garbage enforcement. Ensure regular enforcement of illegal garbage dumping and unsanitary conditions in the alley.
- Parking enforcement. Ensure regular enforcement of no parking policy in the alley.



Stone

Conveniently linked to Trenton 1 and primarily a residential alley, Stone is a pleasant passageway for pedestrians. Adjacent to a number of community facilities, i.e. a hospital, an elementary school annex, and a church, it is relatively cleaner than most

Recommendations - Renovation

alleys.

Key Issues

- Paving improvements could enhance the alley environment for its residents.
- Existing lights are PG&E wood pole lights (2 total) serviced by overhead wires. Adequacy of existing lights should be evaluated.

Physical Improvement Proposals for Stone

Paving

- Concrete Paving. Reconstruct the roadway.

Lighting

- City standard pedestrian lights. Remove existing PG&E lights, underground overhead wires and install new pedestrian scale lights (see page 99 - Lights.)

Traffic

- Sidewalks and curbs. Reconstruct sidewalks and curbs.
- One-way street. Allow vehicular movement one-way from Jackson to Washington as is.
- Bollards. Locate metal pipe bollards on both sidewalks to clearly separate pedestrian zones from the roadway.
- Allow no on-street parking as is.

Other Enhancements

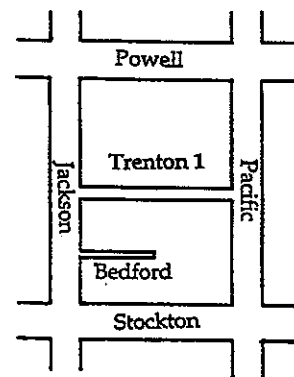
- Alleyway name in English and Chinese imprinted in paving (see page 31 - General Plan.)

Policy Visions for Stone

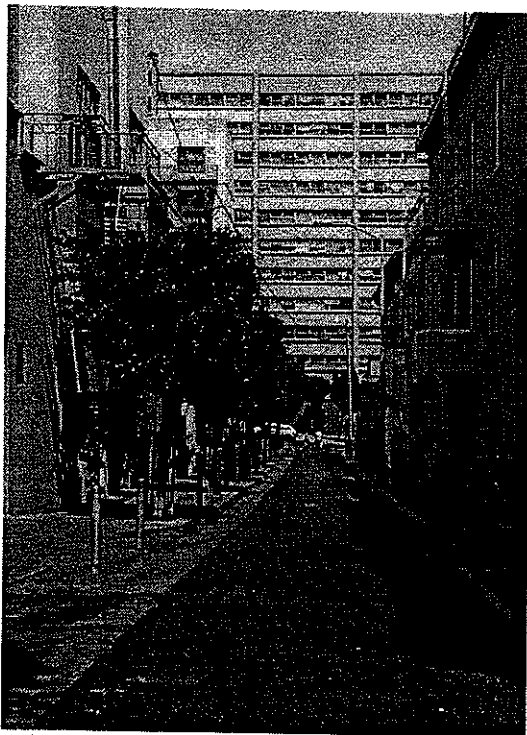
- Street cleaning and sweeping. Ensure regular cleaning and sweeping of the alley.
- Parking enforcement. Ensure regular enforcement of no parking policy in the alley.

Trenton 1

Trenton 1 is one of a few alleys in Chinatown with trees. It is in a fairly good condition and was last renovated in 1984. Trenton 1 is especially unique in that it has a fairly sizable concrete plaza, an extension of the west sidewalk, adjacent to it.



Recommendations - Renovation



*Trenton 1 Alley as Existing:
Looking North from Jackson*

Key Issues

- Re-evaluate the use of the plaza. Illegal parking in the plaza area is common. The homeless typically camp there.
- Although the alley has city standard cobra lights (3 total) and overhead wires serving those lights are undergrounded, two PG&E wood poles (w/o lights) with overhead wires remain.
- Plants in existing planters are not well maintained.

Physical Improvement Proposals for Trenton1

Lighting

- Remove remaining PG&E wood poles and overhead wires.
- City standard pedestrian lights. Remove city standard pole lights and install city standard pedestrian lights (see page 99 - Lights.)

Landscaping

- Evaluate existing plants in planters and replant as necessary.

Traffic

- One-way street. Allow vehicular movement one-way from Jackson to Pacific as is.
- Bollards. Locate concrete seat bollards between the existing pipe bollards at the west sidewalk, adjacent to the plaza, to prevent illegal parking.
- Allow no on-street parking as is.

Other Enhancements

- Alleyway name in English and Chinese imprinted in the paving (see page 31 - General Plan.)

Policy Visions for Trenton 1

- Street cleaning and sweeping. Ensure regular cleaning and sweeping of the alley.
- Garbage enforcement. Ensure regular enforcement of illegal garbage dumping and unsanitary conditions in the alley.
- Parking enforcement. Ensure regular enforcement of no

Recommendations - Renovation

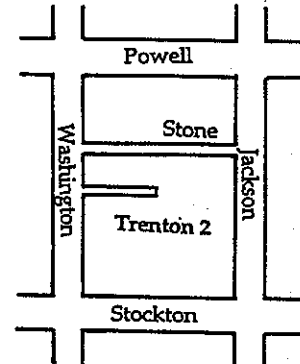
parking policy in the alley.

Trenton 2

Trenton 2 is primarily a back alley commonly utilized as a driveway to a private parking lot located at the end of the alley. It is occupied on the west side by a children's play area, which adjoins the nearby Commodore Stockton Elementary School.

Key Issues

- Children's use of the alley conflicts with the driveway to the back parking area. Although a speed limit sign is present, additional improvements or signs are needed to safely delineate the children's space from cars.
- The one existing light is PG&E wood pole light serviced by overhead wires.



Physical Improvement Proposals for Trenton 2

Lighting

- City standard pole lights. Remove existing PG&E light, underground overhead wires and install new city standard cobra lights (see page 100 - Lights.)

Traffic

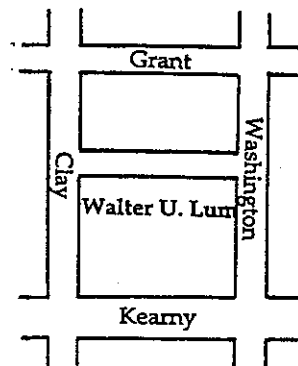
- Install street signs, i.e. a school plate, indicating the children's use of the alley.
- Install paving stripes to warn drivers of children's activities.
- Allow no on-street parking as is.

Walter U. Lum

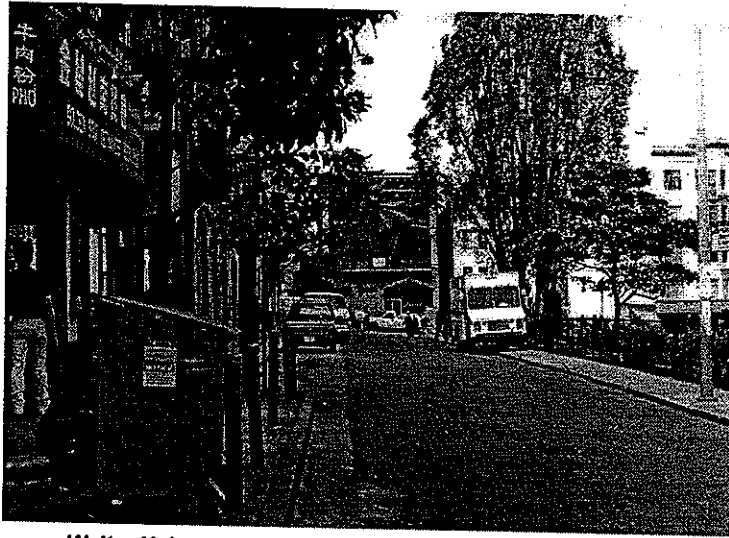
Walter U. Lum Alley is wider than most alleys. It is open, sunny and adjacent to a highly used open space: Portsmouth Square.

Key Issues

- Better enforcement is needed to monitor illegal parking activities, especially along the east sidewalk, to prevent potential blocking of the alley.



Recommendations - Renovation



Walter U. Lum Alley as Existing: Looking North from Clay

Physical Improvement Proposals for Walter U. Lum

- Evaluate and if needed, upgrade existing city standard cobra lights (3 total.)

Traffic

- Bollards. Install closely spaced metal pipe bollards on the east sidewalk.
- Retain on-street metered parking.

Other Enhancements

- Alleyway name in English and Chinese imprinted in paving (see page 31 - General Plan.)

Policy Visions for Walter U. Lum

- Garbage enforcement. Ensure consistent enforcement of illegal garbage dumping and unsanitary conditions in the alley.
- Parking enforcement. Ensure regular enforcement of no parking policy on the east side of the alley.

Phase III Alleys

Brooklyn

Brooklyn is a quiet, dead-end, residential alley with a pedestrian connection to Hang Ah Alley.

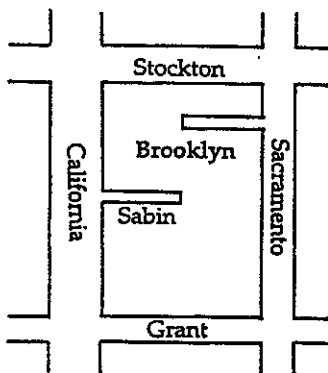
Key Issues

- Better enforcement of illegal activities and general upgrade would enhance this aged alley.
- Existing sidewalks with planters are too narrow for safe pedestrian and wheelchair access.

Physical Improvement Proposals for Brooklyn

Paving

- Asphalt-concrete paving. Resurface the roadway.



Recommendations - Renovation

Lighting

- Evaluate and if needed, upgrade existing city standard cobra lights (2 total.)

Landscaping

- Remove existing planters.

Traffic

- Sidewalks and curbs. Reconstruct sidewalks and curbs.
- Allow no on-street parking as is.

Other Enhancements

- Alleyway name in English and Chinese imprinted in paving (see page 31 - General Plan.)

Policy Visions for Brooklyn

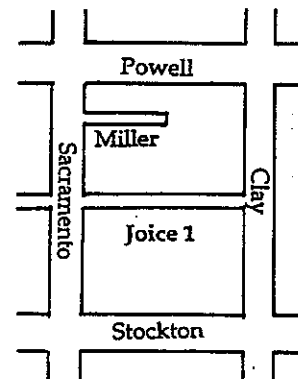
- Street cleaning and sweeping. Ensure regular cleaning and sweeping of the alley.
- Garbage enforcement. Ensure regular enforcement of illegal garbage dumping and unsanitary conditions in the alley.
- Parking enforcement. Ensure regular enforcement of no parking policy in the alley.

Joice 1

Adjacent to the former YWCA and Cameron House, Joice 1 is one of the most pleasant and well used alleys in Chinatown. Last renovated in the mid-1980's, it is in a fairly good condition.

Key Issues

- The alley suffers from illegal garbage dumping and littering.
- Illegal parking also hinders the alley's use as a clear passageway for one-way through traffic.
- Existing lights are PG&E wood pole lights (3 total) serviced by overhead wires.



Physical Improvement Proposals for Joice 1

Paving

- Evaluate and if needed, improve existing paving.

Lighting

- City standard pedestrian lights. Remove existing PG&E lights,

Recommendations - Renovation

underground overhead wires and install new pedestrian scale lights (see page 99 - Lights.)

Traffic

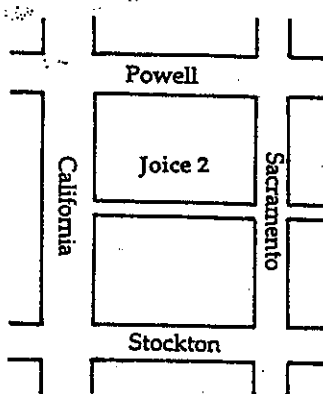
- Evaluate and if needed, reconstruct sidewalks and curbs.
- One-way street signage. Clearly define one-way traffic from Clay to Sacramento with visible street signage.
- Allow no on-street parking as is.

Other Enhancements

- Alleyway name in English and Chinese imprinted in the paving (see page 31 - General Plan.)

Policy Visions for Joice 1

- Street cleaning and sweeping. Ensure regular cleaning and sweeping of the alley.
- Garbage enforcement. Ensure regular enforcement of illegal garbage dumping and unsanitary conditions in the alley.
- Parking and one-way street enforcement. Ensure regular enforcement of no parking and one-way traffic in the alley.



Joice 2

Joice 2 is a residentially oriented, relatively clean and out of the way alley. Linked to Joice 1, it directly connects downtown with Chinatown. The alley is noticeably sloped and is fairly wide.

Key Issues

- Nighttime safety is a primary concern here. Lighting is deemed to be poor at night; residents cite criminal activities and youth loitering. (The alley has two PG&E wood pole lights serviced by overhead wires.)
- Street signage direction is not clear; complaints have been filed in regards to vehicles entering the alley from the wrong side. (The Department of Parking and Traffic's records show that Joice 2 is currently two-way.)
- Speeding in the alley has been observed.

Physical Improvement Proposals for Joice 2

Paving

- Evaluate and if needed, improve existing paving.

Recommendations - Renovation

Lighting

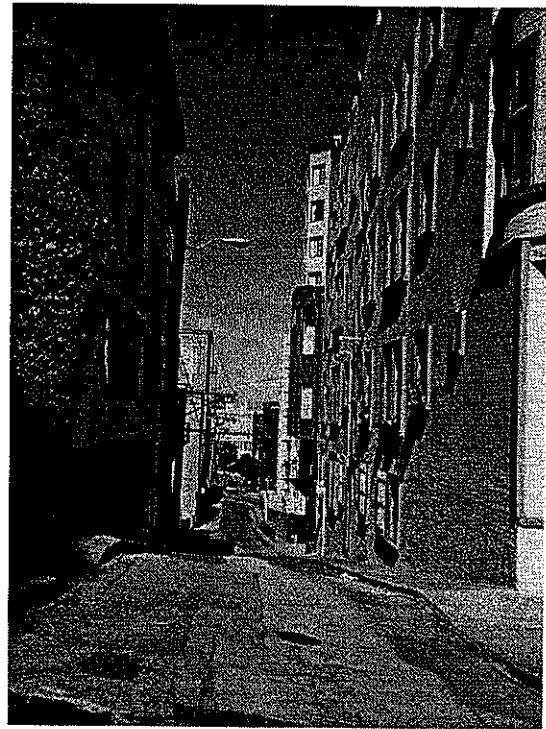
- City standard pedestrian lights. Remove existing PG&E lights, underground overhead wires and install new pedestrian scale lights (see page 99 - Lights.)

Traffic

- Evaluate and if needed, reconstruct existing sidewalks and curbs.
- One-way direction. Establish one-way traffic direction from Sacramento to California and clearly display it with visible signage.
- Allow no on-street parking as is.

Policy Visions for Joice 2

- Street cleaning and sweeping. Ensure regular cleaning and sweeping of the alley.
- Garbage enforcement. Ensure regular enforcement of garbage dumping and unsanitary conditions in the alley.
- Parking enforcement. Ensure regular enforcement of no parking and one-way traffic in the alley.



*Joice 2 as Existing:
Looking North Towards Joice 2 from California*

Miller

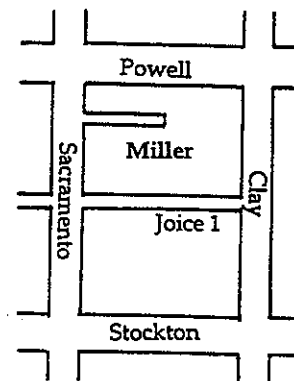
No improvements are proposed for Miller under the Alleyway Master Plan. A well kept, dead-end alley, Miller is primarily used by those who live there. It is the only alleyway evaluated in the *Alleyway Master Plan* that is privately maintained (by a condominium association.)

Parkhurst

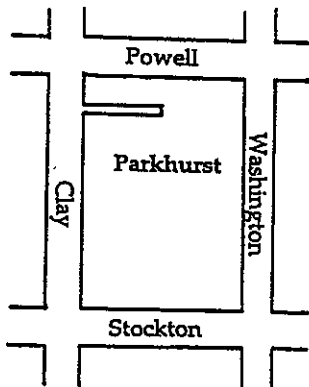
Parkhurst, which is mainly a backside to residential units, is a small, dead-end alley. It is not frequently utilized by pedestrians.

Key Issues

- The existing asphalt-concrete paving has many pot holes and needs improvement.
- The back wall of the adjacent school, which borders the alley on the east side, attracts graffiti. An "alleyway" mural depicting the history of the alleys in Chinatown might be



Recommendations - Renovation



appropriate for this very large and visible wall space.

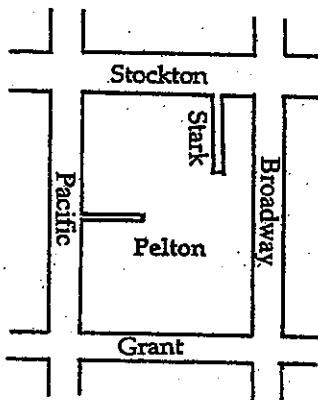
Physical Improvement Proposals for Parkhurst

Paving

- Asphalt-concrete paving. Resurface the entire alley.

Other Enhancements

- Wall mural. Explore the potential of a wall mural at the east wall, adjacent to the Commodore Stockton Elementary School.



Pelton

This alley functions as a short-cut for pedestrians going to and from Broadway; many people go through the Royal Pacific Motel.

Key Issues

- This narrow alley suffers from overspill of garbage generated from nearby businesses.
- No public, street lights exist in the alley. The alley is currently lit by private wall mounted lights.

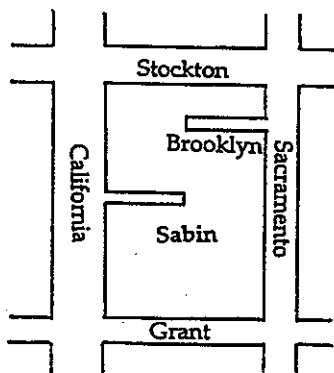
Physical Improvement Proposals for Pelton

Lighting

- Evaluate and improve existing lighting condition.

Policy Visions for Pelton

- Garbage enforcement. Ensure regular enforcement of illegal garbage dumping and unsanitary conditions in the alley.



Sabin

Sabin is a small, out of the way alley. On its east side is an empty office building and on the west, there are two privately operated public parking lots, one of which has an entrance to the alley.

Key Issues

- The alley leads to nowhere right now but has a potential to be connected to an abandoned garden, adjacent to the YMCA.
- The future of the alley depends on the development future of the surrounding properties.

Recommendations - Renovation

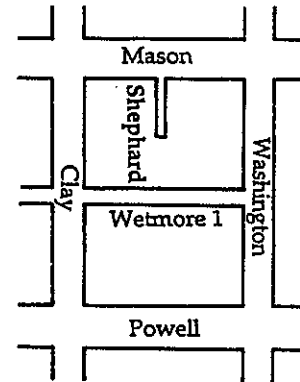
No physical improvement proposals are made for Sabin in the Alleyway Master Plan. However, a long term goal is to explore a better connection from the alley to the YMCA.

Shephard

Shephard is a clean, well kept residential alley. Because it is far from the commercial core of the Chinatown area, it is relatively problem free.

Key Issues

- Better lighting is a concern. There is one existing PG&E wood pole light serviced by overhead wires.
- Better signage and identification of the alley is also a concern.



Physical Improvement Proposals for Shephard

Lighting

- City standard pole lights. Remove the existing PG&E light, underground overhead wires and install city standard cobra lights (see page 100 - Lights.)

Traffic

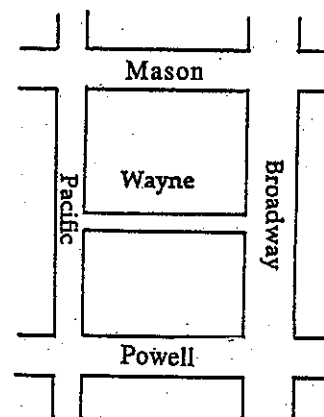
- Evaluate and improve street signage, i.e. place a "Not a Through Street" sign to more prominently identify the alley.

Wayne

Wayne Alley is a fairly clean, uncrowded, quiet, residential alley. It serves as a convenient short-cut for pedestrians going to and from Pacific to Broadway.

Key Issues

- Evaluate the use of the plaza area; it is not very pleasant there with traffic noise from the adjacent Broadway Tunnel. Also, one of the few complaints residents have of the area is the occupancy of the plaza by "unwanted" (homeless) people.
- The northernmost end of the alley at Broadway is meant to be a turn around space, but cars park there illegally; better parking enforcement in the alley is needed.
- Existing lights are PG&E wood pole lights (2 total) serviced by overhead wires; it is observed by residents to be inadequate.



Recommendations - Renovation

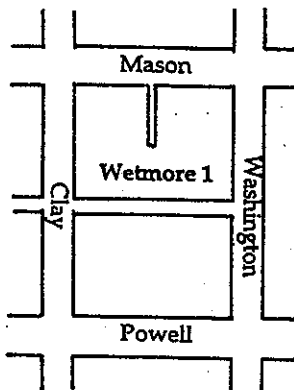
Physical Improvement Proposals for Wayne

Lighting

- City standard pole lights. Remove existing PG&E lights, underground overhead wires and install city standard cobra lights (see page 100 - Lights.)

Policy Visions for Wayne

- Parking enforcement. Ensure regular enforcement of no parking policy in the alley.



Wetmore 1

Wetmore 1 is close enough to the Chinatown core area yet far enough away to enjoy a quiet, residential atmosphere, away from commercial activities. It is in a fairly good condition.

Key Issues

- Existing paving is stone paving covered randomly with asphalt patches.
- The existing one-way traffic direction is not very clear.
- Existing lights are PG&E wood pole lights (2 total) serviced by overhead wires.
- Illegal parking is a problem.

Physical Improvement Proposals for Wetmore 1

Paving

- Asphalt-concrete paving. Resurface the roadway.

Lighting

- City standard pedestrian lights. Remove existing PG&E lights, underground overhead wires and install new pedestrian scale lights (see page 99 - Lights.)

Traffic

- Sidewalks and curbs. Reconstruct sidewalks and curbs.
- One-way street. Allow vehicular movement one-way from Washington to Clay as is.
- One-way street signage. Prominently display one-way traffic direction from Washington to Clay at both ends of the alley.
- Allow no on-street parking as is.

Recommendations - Renovation

Policy Visions for Wetmore 1

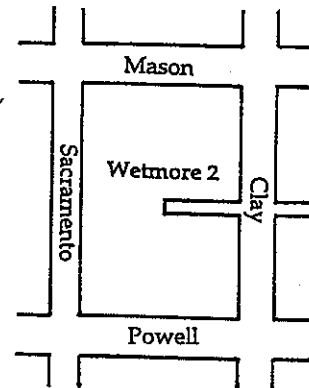
- Parking and one-way street enforcement. Ensure regular enforcement of no parking and one-way traffic in the alley.

Wetmore 2

Across from Wetmore 1 is a dead-ended Wetmore 2, which is significantly sloped with views to the Bay. It is also a residentially oriented alley in a fairly good condition.

Key Issues

- The most significant improvement that the alley needs is in the paving; the existing stone paving has been haphazardly maintained.
- There is one existing PG&E wood pole light serviced by overhead wires.



Physical Improvement Proposal for Wetmore 2

Paving

- Asphalt-concrete paving. Resurface the roadway.

Lighting

- City standard pedestrian lights. Remove existing PG&E lights, underground overhead wires and install new pedestrian scale lights (see page 99 - Lights.)

Traffic

- Sidewalks and curbs. Reconstruct sidewalks and curbs.
- Allow no on-street parking as is.

Policy Visions for Wetmore 2

- Parking enforcement. Ensure regular enforcement of no parking in the alley.

The proposed physical improvements for the thirty-one alleyways are summarized below in the following chart.

Recommendations - Renovation

| | | ROADWAY PAVING | LIGHTING | UNDERGROUNDING | LANDSCAPING | TRAFFIC | PARKING | | SPECIAL PROJECTS | | | | | | | | | | | | |
|------------|---------------|----------------|----------|----------------------------|------------------|--------------|--------------------------------------|-----------------------------|------------------|---|--|-------------------------|-----------|-----------|---------------------------|-------------------------------|----------------------|--------------------|-------------|--|-------------------------|
| | | Concrete | Asphalt | City Standard Pole (Cobra) | Pedestrian Scale | Wall Mounted | Undergrounding of Existing Utilities | Small Tree/Shrub in Planter | Tree in Ground | Limited Vehicular Access to Delivery Trucks | Alley Closed to Traffic (10 a.m. to 4p.m.) | One-way Through Traffic | One Grade | Sidewalks | Metered On-Street Parking | Residential On-Street Parking | No On-Street Parking | Alley Name Imprint | "Footprint" | Other Art Projects, i.e. Wall Plaques, Murals... | No Improvement Proposed |
| PHASE I: | | | | | | | | | | | | | | | | | | | | | |
| 1 | Beckett | X | | | X | | X | | | | | X | X | | | X | X | X | X | X | |
| 2 | Commercial | X | | | X | | | X | | | | X | | X | X | | X | X | | X | |
| 3 | Cordella | X | | | X | | | X | | | | X | | X | | | X | X | X | X | |
| 4 | Hang Ah | | | X | | | | | | | X | | | | | | | | X | X | |
| 5 | John | X | | | X | | X | | X | | | X | | X | | X | X | X | | | |
| 6 | Ross | X | | | X | | | X | | | X | | X | | | | X | X | X | X | |
| 7 | Spofford | X | | | X | | X | X | | | X | | X | | | | X | X | X | X | |
| 8 | Waverly 1 | X | | | X | | X | X | | | | X | | X | | X | | X | X | X | |
| 9 | Waverly 2 | X | | | X | | X | X | | | | X | | X | | X | | X | X | X | |
| 10 | Wentworth | X | | | X | | X | X | | | | X | | X | | X | | X | X | X | |
| PHASE II: | | | | | | | | | | | | | | | | | | | | | |
| 1 | Bedford | X | | | | X | | | | | | | | X | | | | | | | |
| 2 | Cooper | | | | | X | | | | | | | | X | | | | | | | |
| 3 | Jack Kerouac | X | | | X | | | | | X | | | | X | | | X | X | | X | |
| 4 | Jason Court | | | | X | | | | | | | | | X | | | | | | | |
| 5 | St. Louis | | | | X | | | | | | | | | X | | | | X | | | |
| 6 | Stark | | X | X | | | | | | | | | | | | | | X | | | |
| 7 | Stone | X | | | X | | X | | | | | X | | X | | | X | X | | | |
| 8 | Trenton 1 | | | | X | | | | | | | X | | X | | | X | X | | | |
| 9 | Trenton 2 | | | X | | | X | | | | | | | X | | | X | | | | |
| 10 | Walter U. Lum | | | X | | | | | | | | X | | X | X | | X | | | | |
| PHASE III: | | | | | | | | | | | | | | | | | | | | | |
| 1 | Brooklyn | | X | X | | | | | | | | | | X | | | X | | | | |
| 2 | Joice 1 | | | | X | | X | | | | | X | | X | | | X | | | | |
| 3 | Joice 2 | | | | X | | X | | | | | X | | X | | | X | | | | |
| 4 | Miller | | | | | | | | | | | X | | X | | | | | | | |
| 5 | Parkhurst | | X | | | | | | | | | | | | | | | | | | X |
| 6 | Pelton | | | | | | | | | | | | | | | | | | | | X |
| 7 | Sabin | | | | | | | | | | | | | | | | | | | | X |
| 8 | Shephard | | | | X | | X | | | | | | | X | | | | | | | |
| 9 | Wayne | | | | X | | X | | | | | | | X | | | X | | | | |
| 10 | Wetmore 1 | | X | X | | | X | | | | | X | | X | | | X | | | | |
| 11 | Wetmore 2 | | X | X | | | X | | | | | | | X | | | X | | | | |

**Note: Instead of the on-street parking, a green parking zone is recommended for short-term parking (for loading and unloading purposes only).*

*Note: Instead of the on-street parking, a green parking zone is recommended for short-term parking (for loading and unloading purposes only).

Recommendations - Implementation

2. Implementation Plan

The Implementation Plan details the construction schedule, construction budget and current and future funding sources for the three phases of the alleyway renovation projects.

Construction Schedule

The recommended construction schedules for the three phases, as follows, are based on the following assumptions:

- (1) Undergrounding Districts can be established and undergrounding work completed in a timely manner; and
- (2) each construction contract contains multiple alleyways; more than one alley gets renovated at one time (see page 79.)

| Phase I | 1998 | 1999 | 2000 | 2001 | 2002 |
|-------------------|--|--|--|--|-------------------------------------|
| Cordelia | *Design Approved and Out to Bid | *Construction Started and Completed | | | |
| Commercial | *Design Approved and Out to Bid | *Construction Started and Completed | | | |
| Ross | *Design Approved and Out to Bid | *Construction Started and Completed | | | |
| Hang Ah | *Design Approved and Out to Bid | *Construction Started and Completed | | | |
| John | * Establish an Undergrounding District | *Undergrounding Completed *Design Approved and Out to Bid | *Construction Started and Completed | | |
| Spofford | * Establish an Undergrounding District | *Undergrounding Completed *Design Approved and Out to Bid | *Construction Started and Completed | | |
| Waverly 1 | * Establish an Undergrounding District | | *Undergrounding Completed *Design Approved and Out to Bid | *Construction Started and Completed | |
| Waverly 2 | * Establish an Undergrounding District | | *Undergrounding Completed *Design Approved and Out to Bid | *Construction Started and Completed | |
| Beckett | * Establish an Undergrounding District | | | *Undergrounding Completed *Design Approved and Out to Bid | *Construction Started and Completed |
| Wentworth | * Establish an Undergrounding District | | | *Undergrounding Completed *Design Approved and Out to Bid | *Construction Started and Completed |

Recommendations - Implementation

| Phase II | 2002 | 2003 | 2004 | 2005 |
|---------------|---------------------------------------|-------------------------------------|--|-------------------------------------|
| Jack Kerouac | *Design Approved and Out to Bid | *Construction Started and Completed | | |
| Stark | *Design Approved and Out to Bid | *Construction Started and Completed | | |
| Bedford | | *Design Approved and Out to Bid | *Construction Started and Completed | |
| Cooper | | *Design Approved and Out to Bid | *Construction Started and Completed | |
| Jason Court | | *Design Approved and Out to Bid | *Construction Started and Completed | |
| St. Louis | | *Design Approved and Out to Bid | *Construction Started and Completed | |
| Stone | *Establish an Undergrounding District | | *Undergrounding Completed *Design Approved and Out to Bid | *Construction Started and Completed |
| Trenton 1 | | | *Remove Exist. PG&E Poles *Design Approved and Out to Bid | *Construction Started and Completed |
| Trenton 2 | *Establish an Undergrounding District | | *Undergrounding Completed *Design Approved and Out to Bid | *Construction Started and Completed |
| Walter U. Lum | | | *Design Approved and Out to Bid | *Construction Started and Completed |

| Phase III | 2005 | 2006 | 2007 | 2008 |
|-----------|---------------------------------------|--|--|-------------------------------------|
| Brooklyn | *Design Approved and Out to Bid | *Construction Started and Completed | | |
| Joice 1 | *Establish an Undergrounding District | *Undergrounding Completed *Design Approved and Out to Bid | *Construction Started and Completed | |
| Joice 2 | *Establish an Undergrounding District | *Undergrounding Completed *Design Approved and Out to Bid | *Construction Started and Completed | |
| Shephard | *Establish an Undergrounding District | | *Undergrounding Completed *Design Approved and Out to Bid | *Construction Started and Completed |
| Wetmore 1 | *Establish an Undergrounding District | | *Undergrounding Completed *Design Approved and Out to Bid | *Construction Started and Completed |
| Wetmore 2 | *Establish an Undergrounding District | | *Undergrounding Completed *Design Approved and Out to Bid | *Construction Started and Completed |
| Wayne | *Establish an Undergrounding District | | *Undergrounding Completed *Design Approved and Out to Bid | *Construction Started and Completed |
| Parkhurst | | | *Design Approved and Out to Bid | *Construction Started and Completed |
| Sabin | | | | |
| Miller | | | | |
| Pelton | | | | |

Construction Schedule Summary:

| | |
|--------------------------|-----------|
| Phase I (10 alleyways) | 1998-2002 |
| Phase II (10 alleyways) | 2002-2005 |
| Phase III (11 alleyways) | 2005-2008 |

Recommendations - Implementation

Construction Budget

The proposed construction budget is based on the following assumptions:

- (1) the current funding source of 2.3 million dollars - Trammel Crow Funds (see below) is fully utilized;
- (2) undergrounding of utilities is funded by the Rule 20A funds (see page 96 - Lights);
- (3) home rewiring costs associated with Undergrounding Districts are the responsibilities of the affected property owners;
- (4) special art projects (see pages 31 to 32 and 81) are implemented utilizing funds other than the current source (see below);
- (5) the cost for special and city standard street signage is implemented using funds other than the current source (see page 114 - Street Signage);
- (6) unit costs used in the proposed construction budgets are based on 1996 figures and may change at the time of actual construction; and
- (7) recommended construction budgets cover all physical

| Contract | Phase I | Budget* | Contract | Phase II | Budget* | Contract | Phase III | Budget* |
|----------|--------------|----------------|----------|-----------------|--------------|----------|-------------|--------------|
| | 1 Cordella | \$130,000.00 | | 1 Jack Kerouac | \$85,000.00 | | 1 Brooklyn | \$46,000.00 |
| | 1 Commercial | \$210,000.00 | | 1 Stark | \$50,000.00 | | 2 Joice 1 | \$31,000.00 |
| | 1 Ross | \$120,000.00 | | 2 Bedford | \$52,000.00 | | 2 Joice 2 | \$31,000.00 |
| | 1 Hang Ah | \$34,000.00 | | 2 Cooper | \$31,000.00 | | 3 Shephard | \$15,000.00 |
| | 2 John | \$200,000.00 | | 2 Jason Court | \$34,000.00 | | 3 Wetmore 1 | \$45,000.00 |
| | 2 Spofford | \$120,000.00 | | 2 St. Louis | \$37,000.00 | | 3 Wetmore 2 | \$25,000.00 |
| | 3 Waverly 1 | \$200,000.00 | | 3 Stone | \$115,000.00 | | 3 Wayne | \$31,000.00 |
| | 3 Waverly 2 | \$200,000.00 | | 3 Trenton 1 | \$52,000.00 | | 3 Parkhurst | \$14,000.00 |
| | 4 Beckett | \$150,000.00 | | 3 Trenton 2 | \$13,000.00 | | N/A Sabin | |
| | 4 Wentworth | \$135,000.00 | | 3 Walter U. Lum | \$36,000.00 | | N/A Miller | |
| | | | | | | | N/A Pelton | |
| | TOTAL COST | \$1,499,000.00 | | TOTAL COST | \$505,000.00 | | TOTAL COST | \$238,000.00 |

Note: Estimated Construction Budget does not include the cost for undergrounding existing utilities, special art projects (i.e. wall plaques, murals, decorative planters) or street signage. It includes the cost for pedestrian scale light poles and wall mounted lights.

Recommendations - Implementation

improvements, except those noted above, as proposed in the Renovation Plan - see pages 31 to 78.

During Construction: Mitigating Measures to Limit Negative Impacts on Chinatown

Construction of any kind impacts Chinatown negatively given the existing congested conditions. It is critical that the implementation of the alleyway renovation projects have the least possible impact on Chinatown business and residential communities.

The *Alleyway Master Plan* recommends the following measures to be considered before and during construction of the alleyway projects.

- A pre-construction meeting between the City, the contractor and the affected alleyway community takes place before any construction begins to promote clear expectations from all parties.
- Measures should be taken during construction to provide reasonable access for all affected alleyway residents and merchants.
- Measures should be taken during construction to ensure the safety of all pedestrians. For example, the construction area, during non-working hours, should be securely protected.
- All construction should occur in a timely manner, as specified in the contracts. Severe penalties should be in place for those who do not abide.
- During construction, proper and visible bilingual signage should be posted at the job site at several locations to inform the general public of the project, i.e., the description of the project, the expected schedule, etc.

Current and Future Funding Sources

The *Alleyway Master Plan* renovation projects are funded by gas tax and roadway funds. Better known as the "Trammel Crow" funds, named after a private developer who purchased a city site slated originally for a road widening project, the total budget for the Chinatown alleyway renovation projects is \$2,938,000.

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After years of community advocacy led by the Committee for Better Parks & Recreation in Chinatown (CBPRC) and the Chinatown Alleyway Improvement Association (CAIA), a Board of Supervisors' resolution in 1994 appropriated the Trammel Crow funds for Chinatown's alleyways. The funds are currently managed by the Department of Public Works and are divided as follows for each step of the renovation projects.

| | |
|-------------------------|-----------------|
| Planning | \$110,000 |
| Design & Engineering | \$264,000 |
| Construction | \$2,300,000 |
| Construction Management | \$264,000 |
| Total Program Cost | \$2,938,000 |

Given the current cost estimates, the construction budget of \$2,300,000 is expected to be completely utilized for the renovation of the thirty-one alleyways. Additionally, it is reasonable to assume that additional funds will be required. Inflation may cause current cost estimates to change in the future, whereby Phase II and Phase III alleys may not get fully funded under the current source. There are also restrictions on the types of improvements that gas tax and roadway funds legally permit. For instance, gas tax and roadway funds cannot be used for the implementation of decorative elements, i.e. art projects or decorative lights (proposed pedestrian lights are city standards and not decorative and therefore qualify for gas tax spending.)

Additional funding, other than the current source, is an apparent need. To effectively leverage current funds, the *Alleyway Master Plan* highly recommends that other funding sources be sought, particularly to implement certain types of improvements, i.e. "footprints," wall plaques, special ground imprints, murals, etc, which legally cannot be implemented under the current source.

The following funding sources, some of which have been utilized in the past for Chinatown alleyway improvements, may be appropriate for the proposed renovation work:

- National Endowment for the Arts;

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- National Historic District Funds;
- Community Development Block Grant;
- Transportation Related (pedestrian improvement) Funds;
- Hotel Tax Funds; and
- Private Donations and Community Sponsorships, i.e. individual donations, family associations, merchants associations, and other community organizations.

3. Maintenance Plan

More than any other element in the *Alleyway Master Plan*, maintenance is key to the success of the alleyway renovation projects. The majority of the *Alleyway Master Plan* survey participants measured their likes and dislikes of Chinatown's alleyways based on cleanliness, and on how well the alleyway environments are kept. When asked of the kinds of improvements the community wished to see in the alleys, keeping the alleys clean was the most common response (see Volume II of the *Chinatown Alleyway Master Plan*.)

Physically maintaining Chinatown's infrastructure at an acceptable level for most people is a challenging task. The challenge in the alleys is even greater because the alleys are less visible, smaller and therefore more difficult to handle than the main streets. Despite the challenges that exist, for the alleyway renovation projects to be successful long after they are completed, serious considerations must be given to how and by whom the alleyways are to be cleaned, monitored and maintained. The Maintenance Plan identifies the objectives required to ensure that the quality of the renovated alleys remain at a level fitting of how the alleys are used today and likely to be in the future.

Two basic premises of the Maintenance Plan guide the recommendations set forth herein. They are:

- (1) Proper and regular maintenance is the most critical renovation improvement that can be made for all thirty-one alleyways; and
- (2) Alleyways' maintenance can only be successfully accomplished and sustained through the joint efforts of both the City and the community.

Maintenance in the alleys can be defined to cover the following: street sweeping, garbage containment and disposal, street repair and environmental monitoring.



Adopt-An-Alleyway Youth at Work

Existing Conditions

Street Sweeping Services

- As "dedicated" and "accepted" streets (see page 102 - Ownership and Responsibility) all thirty-one alleyways under the *Alleyway Master Plan*, except for Ross Alley, which is dedicated but not accepted for maintenance, currently should receive regular street cleaning services from the City. Although this is the case, the *Alleyway Master Plan* observations and surveys indicate that regular street sweeping services are lacking in the majority of the alleys (see Volume II of the *Chinatown Alleyway Master Plan*.)
- To supplement city services, local youth volunteers, the Adopt-An-Alleyway (AAA) Youth Project, monitor and sweep clean alleys every other month for six times a year. Supported by the Neighborhood Beautification Funds, AAA is aided by the Department of Public Works' broom brigade.

Garbage Containment and Disposal

- By law (Municipal Health Code, Section 283), the City requires that all residential and commercial garbage be contained in a city approved, covered garbage container for disposal. The law (Municipal Public Works Code, Section 170) also requires that garbage be stored on one's own premise until 6 p.m. of the night of the garbage collection service. The majority of the alleyway residents and merchants are not aware of and/or do not abide by such regulations (see Volume II of the *Chinatown Alleyway Master Plan*.)
- A number of alleys have public garbage cans. In almost every case, public garbage cans are more deterrents than aids in keeping the alleyway environments litter free. Observations and surveys show that public garbage cans are targets of illegal garbage dumpings; overspillage of the cans result when people illegally put large quantities of private garbage into these cans.
- A few alleyways still have cubic yard bins or commercial garbage dumpsters; their permanent presence in the alleys is illegal (Municipal Public Works Code, Section 170.) The

Recommendations - Maintenance

dumpsters are almost always left opened and/or unlocked, allowing pedestrians to go into them. Soon after they are put in the alleys by merchants, the areas around the dumpsters quickly become very dirty and unsanitary, lessening the livability of the alleyway environments.

- All residents by law are required to have a paid garbage collection service. Some in the community express no knowledge of such a requirement and dispose their own garbage in others' and/or in public garbage cans (see Volume II of the *Chinatown Alleyway Master Plan*.) This causes numerous litter problems in the alleys. The problem is compounded when the disposal company does not collect all garbage that gets put out, leaving opened bags behind.
- Some in the community who do not have garbage collection service and/or have extra garbage typically have designated spaces in alleys where they illegally dump, such as a building corner, as in Spofford and/or at a public plaza, such as at Trenton. Dumped garbage often sits in the alleys indefinitely until the service is called for by a resident and/or by pedestrians.



Illegally Dumped Garbage at Pelton Alley

Street Repairs

- Because all but one of the thirty-one alleys under the *Alleyway Master Plan* are "dedicated" and "accepted" streets (see page 102 - Ownership and Responsibility), the City is responsible for the repair of roadway deteriorations, damage to street lights, other street furnishings, etc. A standard exception occurs for the sidewalk area where it is the jurisdiction of the adjoining property owner to do such repairs, i.e. sidewalk paving, damage to any street furnishings or trees on the sidewalks sponsored by the adjoining property owner. Currently, it is not very clear to those in the alleys as to who does which repair. Community residents and merchants assume the City to be

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responsible for most, if not, all repairs. Also, because few records are kept where the alleyways are concerned, responsible city agencies unintentionally neglect alley environments.

Monitoring of Alleyway Conditions

- In addition to sweeping Chinatown's alleyways, youth volunteers who make up the Adopt-An-Alleyway Youth Project (AAA) regularly monitor Chinatown's alleyways. They are the only entity present currently who observe the alleyway conditions on an on-going basis. Quarterly each year, the youth assign "grades" to each alley based on how well kept and clean the alleys are, and publish them in local papers for greater community awareness.

Alleyway Master Plan Recommendations

1. Increase the Capacity of the Adopt-An-Alleyway Youth Project (AAA):

AAA is one proven program that works for the alleyways. It has a potential to expand, given the dedication and the interest of the youth volunteers. With increased capacity, AAA would be able to provide greater monitoring and cleaning services to the alley environments, thus prolonging the life of the renovation projects. AAA's expansion could also potentially benefit the youth, by providing opportunities for leadership, community work and eventually, the potential of an income.

The Alleyway Master Plan's vision for AAA's expansion is for it to become, over a period of time, more of a self-sustaining **job training program**. Under this program, some of the youth volunteers would be paid employees who would themselves run the program like a business. The proceeds or the income from the program would be recycled and put back into the program, thus becoming a self-sustained operation. This model is largely based on a work recently commissioned by CRC and completed by a U.C. Berkeley graduate student, entitled "San Francisco Chinatown Alleyway Master Plan: An Economic Development Element" (see page 120 - References.)

Recommendations - Maintenance

There are several ways in which such a program could be funded. Initially, it would need start-up monies through a grant, such as a Community Development Block Grant, or through a private sponsorship. In the first couple of years, it is not expected to be self-sustaining and will, therefore, need outside financial support. However, once the program is established and has a community-wide, marketing campaign in place, it can begin to generate funds on its own through private donations from the community, particularly the alleyway constituents.

2. Provide Community Education and Awareness:

Illegal acts committed in the alleys in regards to cleanliness and maintenance issues are mostly committed out of a lack of respect for the environment and a lack of knowledge on the part of the perpetrators. Respect for the alleyway environments must be fostered; only out of respect would there be a sense of caring and community ownership.

The *Alleyway Master Plan* recommends that, with the start of the renovation work, a **community-wide marketing campaign**, highlighting Chinatown alleyways' assets be launched. The introduction of the *Alleyway Master Plan* to the larger community, celebrating the completion of the report and the start of the renovation projects, can be a basis for such a campaign. Through media, newspaper press and via an advertising campaign, the community would see the benefits of the renovation projects and the critical importance of the role that community members can play in the maintenance of the alleys.

Community education work does not and should not end with one marketing campaign. Continuous efforts must be made to continue to foster community awareness of the alleyway environments. As an effort that could continue indefinitely, the *Alleyway Master Plan* recommends the development of **an educational brochure** around maintenance issues. The brochure could potentially highlight cleaning and garbage services that are currently available, how one could access them, and whom one would contact if the services are lacking or not regular. The brochure should also include basic information on

Recommendations - Maintenance

cleanliness in regards to the alleyway environments, for example, the do's and don'ts of garbage disposal (also see page 94 - Community Education Plan.)

3. Evaluate and Upgrade Existing City Services for the Alleyway Community:

It is unforeseeable for Chinatown's alleyways to become completely privatized and privately maintained. City services are not only a right but essential to Chinatown's alleyways. Services, shown to be inadequate, must be brought up to a level where they should be and increased, if needed.

The *Alleyway Master Plan* recommends an **evaluation of existing city services**, to be conducted jointly by a committee made up of community and city representatives. A set of recommendations aimed at improving existing city services is to be reached at the conclusion of the evaluations. The discussions should center around what works and what doesn't and on how the existing city services could be better tailored to Chinatown's alleyways and to the unique needs of the alleyways' merchants, residents and property owners. For example, it has been noted that garbage pick-up times in certain alleyways are not appropriate for the residents who live there (see page 39.) The City, the community and the garbage disposal company should come together to find a solution, on a case by case basis, to meet everyone's needs.

4. Expand the Golden Gate Disposal's existing neighborhood collection service:

Illegal dumping of large appliances, i.e. refrigerators, television sets, bed frames and mattresses, is commonly found in alleyways, typically at the expense of alleyway residents and users. To curtail such activities, the free, twice yearly neighborhood collection service provided by the Golden Gate Disposal (see page 107 - Cleaning Issues) should be expanded.

5. Encourage Recycling in the Community:

Recycling services are almost non-existent in Chinatown, unlike the rest of the City. Yet, recycling is one opportunity for the

Recommendations - Maintenance

community to legally dispose of unwanted goods. **Curbside pick-up** in Chinatown for recycling is highly recommended.

Recycling is also an educational opportunity for the Chinatown alleyway community. People should be made aware of the benefits of recycling and the neighborhood services that are available, such as a free collection of large items twice yearly (see page 107 - Cleaning Issues.)

6. Post ("No Dumping", "No Feeding the Pigeons", "No Littering") Bilingual Signs:

Warning signs to deter people from committing acts detrimental to the alleyway environments are needed. Each alley, where appropriate should be posted with bilingual signs indicating no dumping, no pigeon feeding and no littering. Additionally, if the signs are to be effective, each warning should be followed promptly with an enforcement action (see page 90 - Enforcement Plan.)

7. Eliminate Public Garbage Cans from All Alleys:

Public garbage cans are more harmful to alleyway environments than do they help solve the littering problem. They should not be placed in Chinatown's alleyways.

4. Enforcement Plan

Improvements to Chinatown's alleyways are not complete until enforcement issues are addressed. Many of the negative attributes of the alleys are a direct result of being without proper enforcement. Whether it is the pedestrian environment impacted because of illegal parking on the sidewalks or unsanitary conditions caused by illegal garbage dumpings, proper and regular enforcement by the City is critical. At the same time, community education should be a part of the continuous efforts on enforcement.

By enforcement, there are specifically two types of regulations most relevant to Chinatown's alleyways: illegal traffic activities and illegal garbage disposal.

Existing Conditions

Unlawful Traffic Activities



Double Parked Cars at Wentworth Alley

- Illegal parking which includes double parking, parking on the sidewalks, and parking where it is not allowed are all deterrents to a pleasant and safe passage of all alleyways. Although illegal parking is a common problem throughout the Chinatown area, the impact is greater when an illegally parked car sits in a narrow alley, completely blocking other vehicles' access as well as pedestrians'.
- Another type of traffic violation that is common in alleys is unlawful vehicular entering. Drivers have been observed to enter alleys from the wrong sides of mostly one-way alleys. It is also typical to see vehicles enter an alley where it is limited to delivery trucks and emergency vehicles only.
- Speeding in alleys is another issue that should consistently enforced. In alleys where pedestrian volume is low, drivers have been observed to speed, endangering pedestrians. A number of close traffic accidents were reported to have occurred at Joice 1 and 2.

Recommendations - Enforcement

Unlawful Garbage Disposal

- Illegal garbage dumping, as outlined in detail under the Maintenance Plan (see pages 84 to 85), is a problem that is not completely unavoidable in a mixed-use, high density community such as Chinatown. However, because illegal dumping is one of the most problematic issues facing Chinatown's alleyways, it is a goal of the *Alleyway Master Plan* to see that it is alleviated.
- Cleanliness problems also arise in alleys when proper garbage containment is not followed. If residents and businesses use lockable garbage cans as required by law, and not bags that can easily get opened, garbage problems in alleyways could be half way resolved.
- Every tenant in Chinatown is required by law to have a garbage collection service (property owners should provide that service.) Additionally, all residences and businesses also have a right to special pick-ups of large items, i.e. mattresses, appliances, etc., upon request (see page 107 - Cleaning Issues.) These facts should be made known to the public and enforced. Unsanitary and unhealthy conditions result when residents and businesses improperly dispose of their own garbage in the alleys.



A Do Not Litter or Dump Garbage Warning Sign

Alleyway Master Plan Recommendations

1. Provide Community Education and Awareness:

Just as people are not mindful of maintenance issues, existing city codes and requirements are ignored or not followed by the alleyway users, because very little respect exist for alleyway environments. Intentionally or unintentionally, alleys are taken for granted and people view them as places where illegal acts can occur.

A **community education campaign** to spread information on the benefits of proper enforcement, particularly emphasizing the community's responsibility, is key to the success of the renovation projects. Similar to the maintenance brochure, an **enforcement booklet** for community-wide distribution is

Recommendations - Enforcement

recommended by the *Alleyway Master Plan*. Currently, a number of existing information booklets on enforcement are available. The Department of Public Works and the Department of Public Health have in the past produced flyers covering general enforcement information and city codes. Community and non-profit groups are also good sources for such information.

2. Improve City's Enforcement Services:

Inadequate city services should be evaluated. Easier said than done, enforcement works best when there is a continual presence of enforcement officers; even just the perception of uniformed officers' presence could greatly curtail illegal activities. Collective efforts must be made by responsible city departments to ensure that Chinatown's alleyways are not excluded from their respective enforcement efforts in Chinatown.

To ensure the success of the City's efforts, the community must continue to work closely with the responsible city agencies. This means reviewing enforcement problems on a case by case basis, as needed, in a team effort to resolve issues. Every small success could eventually do well for the larger alleyway program and in turn, for the Chinatown community and the City.

3. Warnings Should be Promptly Followed with Penalties:

Warning citations without penalties are not effective in a mixed-use Chinatown community where residences and businesses co-exist side by side. Careful attention should be given to enforcement actions that follow warning citations. Also, for warning citations to be more effective, penalties or monetary fines given should be relative to the illegal act committed. The *Alleyway Master Plan* recommends a progressive increase in fines as the number of illegal acts committed increase.

4. Provide Close Attention to Alleys with Limited and/or No Vehicular Access:

A number of alleys in the *Alleyway Master Plan* are planned for street closures and for limited vehicular access (see pages 109

Recommendations - Enforcement

to 110 - Access.) Those alleys, especially, should receive closer enforcement attention. Ross, Spofford and Hang Ah are proposed for street closures and Jack Kerouac and Wentworth are respectively recommended for delivery trucks access and short-term loading and unloading only.

5. On-Street Parking Enforcement:

On-street parking does not benefit the alleyway community unless it is properly enforced. Proper and consistent enforcement of on-street parking, i.e. residential permit, metered parking and loading zones, is a requirement in all affected alleyways.

5. Community Education Plan

Community education touches all elements of the *Alleyway Master Plan*: renovation, implementation, maintenance and enforcement. Community awareness of and support in the work to be accomplished is critical because without community participation, the long term success of any of the proposed plans is doubtful.

Of all the recommendations in the *Alleyway Master Plan*, the Maintenance and Enforcement Plans in particular require that alleyway residents, merchants, property owners and the larger Chinatown community actively participate to accomplish the tasks proposed. For the Maintenance Plan to be a success, the community must first see it as beneficial to them to do what they can to keep the alleyways clean and once aware of the benefits, they must act. The Enforcement Plan similarly requires active community involvement; community members must be first informed of existing laws and city codes surrounding enforcement issues and then follow them or propose changes as needed for the benefit of the alleyways and in turn, the larger Chinatown community.

How one reaches the Chinatown community, on the other hand, is a tougher question to answer. As in any neighborhood, Chinatown is made up of a diverse population of individuals with different backgrounds and needs. This complexity is compounded by the fact that Chinatown is also a mixed-use community: a neighborhood of closely-knit businesses and residences. The community education component, in this context is not only necessary but extremely challenging.

The Alleyway Master Plan recommends the following measures for community involvement:

- The leader for community education efforts should be most appropriately the Chinatown Alleyway Improvement Association (CAIA), because it is the most established community based organization in Chinatown specifically concerned historically and presently with the welfare of the

Recommendations - Education

alleyway environments. It is also the only organization in Chinatown with an alleyway community membership.

The *Alleyway Master Plan* recommends the **expansion of CAIA's membership** to include, more specifically, community members associated with Phase I and II alleys (see pages 27 to 28.) The inclusion of the specific alleyway residents, merchants and property owners to CAIA is very much needed, because community education efforts should not be a one time effort. Community education is a continuous campaign with a message that could only be most effectively transferred from community members to community members.

- CAIA's leadership in community education efforts can and should be supported by the also soon to be expanded Adopt-An-Alleyway Youth Project (AAA.) Specifically in the direct community outreach and education efforts, AAA youth leaders could play a critical role.

Specifically, the following areas under the Alleyway Master Plan require community education and organizing efforts:

- Undergrounding of existing utilities: Undergrounding Districts (see page 96 - Lights.)
- Proper garbage disposal and containment (see page 107 - Cleaning Issues and see pages 83 to 85 - Maintenance Plan.)
- Limited traffic access or street closures in the alleys (see pages 109 to 110 - Access.)
- Proper traffic movement and parking in the alleys (see pages 33 to 75 - Renovation Plan and see page 112 - Parking.)

A. Lights

Lighting, next to cleanliness, is the most commonly expressed concern of alleyway residents, merchants and property owners. Two issues surrounding lights affect Chinatown's alleyways: Undergrounding Districts and the use of pedestrian oriented or pedestrian scale lights.

1. Undergrounding District

Background

Undergrounding of existing utilities is an issue for many of Phase I alleyways, namely Beckett, Wentworth, Waverly 1 and 2, Spofford and John. Other alleys, which are of a less priority, but also with overhead wires are Joice 1 & 2, Shepherd, Stone, Trenton 1 & 2, Wetmore 1 & 2 and Wayne. (A goal of the *Alleyway Master Plan* is to see that undergrounding of existing overhead wires, where possible, be completed before the start of any improvement work for Phase I alleyways (see pages 77 to 78 - Implementation Plan.) Street lighting design, however, is recommended to be completed concurrently or before the undergrounding work begins so that any service points or utility boxes required of street lights can be installed at the same time as the undergrounding work.)

It is relevant to look at undergrounding of existing utilities in alleyways for a number of reasons. The exposed utilities offer a threat to pedestrians and residents, especially in an emergency. They are also unsightly and are visually distracting. More importantly, there is a practical reason why the utilities should be undergrounded: the existing lights are insufficient. PG&E lights do not sufficiently light the ground, where it is most needed. One of the major complaints that residents have across all alleyways observed is the inadequacy of lights at night. People point to the lack of good lighting as a primary cause of numerous criminal acts and thefts observed in the alleys. An *Alleyway Master Plan* objective is to have lights that are more closely spaced and are at pedestrian scale so that there is an even

distribution of lights at the street level, providing safety and security for the residents as well as for passing vehicles.

Players and the Process

Rule 20A exists as a resource for the community to use when undergrounding existing PG&E lights and telephone utilities. A declaration of an "Undergrounding District", under Rule 20A, essentially provides for the undergrounding of existing overhead utilities in San Francisco at the expense of the City. **PG&E**, in an agreement with the City, called Rule 20A, funds and implements Undergrounding Districts (there is approximately 4 million dollars available each year city-wide) and the **Bureau of Street Use and Mapping (BSM) under the Department of Public Works (DPW)** administers the undergrounding program. DPW does the street lighting design and oversee the construction process.

The undergrounding, which includes trenching and burying of overhead utilities, is paid for by funds under the Rule 20A. Once the PG&E wood poles are removed and overhead wires are undergrounded by PG&E, the City installs city accepted light poles and fixtures. In most cases, the installed costs of the lights are assessed on the adjoining property owners through a Special Assessment District. The cost to the property owner is estimated by the City to be approximately \$1,500 to \$2,500 per light installed. (Under the *Alleyway Master Plan*, the cost of the new street lights is to be borne by the renovation projects and not to be assessed on the property owners, provided that there are adequate funds - see page 79 - Implementation Plan.) Once the new lights are in place, PG&E becomes the service provider only and is no longer liable nor responsible for the maintenance of the lights. The City maintains the newly installed lights.

The primary role of a property owner in an Undergrounding District is to rewire the home electrical lines to accept the newly undergrounded wiring. The cost for rewiring home connections vary but it is estimated by the City to be approximately \$1,500 per single family dwelling. In Chinatown, the rewiring costs for the property owners may be higher since many buildings have multiple units and are old. A recent estimate completed by the

Chinatown Community Development Center for a Beckett Alley residence, a typical Chinatown alley building, costs approximately \$4,000 (estimate provided by the Yick Electric Company, Inc., San Francisco, 1994.) For the property owners expressing hardship, there is a program entitled **Code Enforcement and Rehabilitation Fund (CERF)**, managed the Mayor's office, which gives out no interest loans to property owners to bring buildings up to code. Low income property owners responsible for home rewiring costs may qualify for such a loan.

Another utility company often involved in Undergrounding Districts is **Pacific Bell** (Pac Bell) and **TCI** (cable). Unlike PG&E, Pac Bell and TCI currently do not have funds set aside to underground their own lines. PG&E, in this case, becomes the main contractor and asks for Pac Bell's and TCI's cooperation. Pac Bell and TCI are also billed for the work done by PG&E, if PG&E does the work with one contractor (often this is the case.) Otherwise, Pac Bell and TCI come in with their own contractor to do their portion of the undergrounding.

More specifically, once the undergrounding is complete, the new city accepted lights are installed and maintained by the **Bureau of Light, Heat and Power (BLHP) under the Public Utilities Commission (PUC-Hetch Hetchy Water & Power.)** It is critical at this stage that city accepted light poles and fixtures are used; otherwise BLHP does not maintain the lights nor accept liability for them. (BLHP also typically reviews DPW's street lighting design prior to implementation.) As of this report, BLHP is working with city departments and community groups to redefine the standards and set design guidelines for pedestrian lighting city-wide.

The required procedure for an Undergrounding District can be summarized as follows:

- A petition by the majority of property owners (at least 51%) is submitted.
- BSM files the petition and initiates an Undergrounding District.
- The Undergrounding District is reviewed at a number of public

hearings by the Board of Supervisors.

- The Underground District is legislated.
- PG&E starts the engineering work.
- Property owners start the home rewiring work.
- Private properties' conversion work is complete.
- PG&E and Pac Bell underground their lines.
- BLHP or DPW installs the new street lights.
- BLHP maintains the new street lights.

After legislation, which takes at least three months, undergrounding work involves one year for design, to prepare construction documents and to process contract bid and award. Once construction starts, the construction period varies but DPW estimates that undergrounding will take approximately six months per alley in Chinatown.

Currently there are ten legislated Undergrounding Districts in San Francisco. Petitions have been received for an additional eight districts and construction is underway on four. (A 1997 city resolution, a master agreement between the City and PG&E, mandates the undergrounding of 42 miles of city streets in the next five years. The Chinatown alleyways are not included in the 42 miles.)

2. Pedestrian Lighting

Background

Pedestrian scale lights or light poles twenty feet tall or less are highly desirable in the alleyway environments because they are better suited for the narrow alleys. The small scale of the pedestrian lights require closer spacing and therefore more poles. As a result, the general perception is that they illuminate better than the taller PG&E wood pole lights or the city standard cobra head lights. (Many lighting experts argue that there is no difference in the amount of light put out by different height light poles.)

The installation of city standard pedestrian scale lights will be a

priority for the alleyway renovations. Adequate night lighting (and/or the perception of adequate lighting) is highly desirable in the alleys, because many of the alleys in question are heavily used residential alleys, where pedestrians frequent after dark. When lighting is inadequate, alley environments become refuges and areas of opportunity for criminals. Crimes in alleys have been cited numerous times by those surveyed in the community.

In about half of the alleys under study (15 total), where there are overhead wires, Undergrounding Districts (see page 96) would be a necessity before pedestrian scale lights can be installed. For the rest of the alleys where utilities are already undergrounded, the installation of pedestrian scale lights would require less of a process.

Players and the Process

In the City, street light installation and maintenance fall under the jurisdiction of the **Public Utilities Commission (PUC-Hetch Hetchy Water & Power.)** The **Bureau of Light, Heat and Power (BLHP)** carries out the PUC mandate to provide adequate lighting for the streets of the City and County.

Currently, PUC allows three types of street lights to be used city-wide. One is the 30 feet tall overhead, cobra head type fixtures with a metal or marble-like pole. This type of lighting extends out over the street and requires fewer poles. The second type, called the post-top units, are 15 to 18 feet tall with a straight pole and a single globe on top. They do not extend out over the street. More poles are required per block and more light shines upward and outward from these units than the standard. And lastly, PUC allows the use of wall mounted lights (see page 58) for narrow streets and alleys where there is no room for standard street poles.

PUC's Policy on Street Lights (8/15/94) states that the "preferred method of street lighting is through the use of standard overhead, "cobra head" type fixtures and, where practicable every effort should be made, by joint use with other

agencies, to utilize a minimum number of poles on a street." The same policy also states that "neighborhoods desirous of post-top fixtures (non-standards) can select from a number of standard designs using high pressure sodium lamps. If the community agrees to pay the differential cost of the system (costs of the lights installed) through a Special Assessment District or other method, staff can approve this deviation from standard practices." Any deviation from PUC policy would require PUC approval. (In alleyways, where there is typically a strong dominance of varied architecture, facade colors and signage, light poles that do not dominate but blend into the environment are best. Therefore, the *Alleyway Master Plan* recommends PUC (City) approved pedestrian scale light poles or wall mounted lights.)

Additionally, under the PUC resolution 93-0172, June 8, 1993, a policy is adopted for the use of high pressure sodium vapor lights as a preferred lighting source for all public outdoor lighting facilities. However, there is now a city-wide movement towards switching to metal halide, a brighter, bluer light in the near future, provided that its life span technology catches up with high pressure sodium lamps, which currently last longer. (The *Alleyway Master Plan*'s intention is to review the use of high pressure sodium lamps or metal halide lamps on a case by case basis. Some alleys may warrant warmer, yellower lights of high pressure sodium fixtures while others may benefit from brighter metal halides. Each case should be evaluated on its own, depending upon adjacent land uses, quality of lighting desired, etc. and after further consultation with BLHP.)

B. Ownership and Responsibility

All but one of the thirty-one alleyways are currently "dedicated" or public streets and are "accepted" for maintenance. To maintain this status, which is critical for Chinatown's alleyways, because it is not foreseeable for them to be privately owned and maintained, the renovated projects would have to be approved by the City for continued dedicated and accepted status. In this section, issues regarding ownership and responsibility of alleyways are discussed. Also explained are the processes surrounding the placement of public or special art projects in public right-of-ways.

1. Encroachment

Background

Streets (and alleys) in San Francisco are either dedicated or private. A "dedicated" street is under public ownership, but the maintenance of it could fall under the City's jurisdiction or under property owners' (properties facing street) responsibility. The City maintains a street only if it meets certain standard requirements (see page 120 - References), and if it does, it is called an "accepted" street. If a street is not accepted, it is the private responsibility to maintain a dedicated street. A "private" street, on the other hand, is completely under a private ownership and the City has no responsibility to maintain it nor is it liable.

The exception to the above is the sidewalk area. Generally in San Francisco, sidewalks immediately adjacent to one's property is the responsibility of the property owner whether it is a dedicated and an accepted street or a private street. "Responsibility" entails repair, sweeping and liability. Sweeping is the exception, where the occupant, not the property owner, is responsible if the property is not vacant. In all other cases, the property owner is the key person responsible for maintaining the sidewalk area.

All alleys but one, Ross Alley, are currently dedicated streets and are accepted for maintenance by the City. This

implies that thirty alleys under the *Alleyway Master Plan* currently should receive city services in regards to cleaning and sweeping and that the City holds liability for hazards incurred in the public right-of-way, outside of the sidewalks, in most, if not all, of those alleys. Ross Alley currently is recognized as a dedicated street but is not accepted. (An *Alleyway Master Plan* goal is to see that all renovated projects, including Ross Alley, are accepted for maintenance (sweeping/cleaning.) It is also a goal of the *Alleyway Master Plan* to see that, where possible, majority of the thirty-one alleyways remain accepted for liability and repair.)

Players and the Process

The **Department of Public Works (DPW)**, specifically the **Bureau of Street Use and Mapping (BSM)**, administers the encroachment permit process. It determines the eligibility and keeps records of streets' ownership and acceptance.

All projects involving public right-of-ways must be approved by BSM to continue to attain dedicated and accepted status. BSM's main functions are to review roadway and road encroachment projects, and to issue permits and clearances. It also coordinates interfaces with various city agencies, particularly in regards to street maintenance and urban forestry.

BSM looks at street acceptance in two ways: one for maintenance (sweeping/cleaning) and one for liability and repair. Whether a project meets these two levels of acceptance are reviewed on a case by case basis. The two types of acceptance are not necessarily mutually exclusive. A street or an alley that is accepted for maintenance may not be accepted for liability and repair and vice versa. In such cases, the adjoining property owners or another party take responsibility for the liability of and for repairing paving materials, planters, etc. that may be in the public right-of-way or on the sidewalk, despite the fact that the City sweeps and cleans the street.

The determination of street or alley acceptance for liability and repair is made primarily through encroachment permits, with some exceptions (see below.) Encroachment permits are

Appendix - Ownership

needed for any non-standard items placed in the sidewalks and in the public-right-of-ways.

The following is a standard procedure for encroachment permits:

- The project sponsor initiates the encroachment permit process by approaching BSM.
- BSM reviews the project internally and with other city departments at the Interdepartmental Staff Committee on Traffic and Transportation (**ISCOTT**.)
- Interdepartmental review is completed and an approval is recommended.
- Legislation by the Board of Supervisors.
- Legislation signed by the Mayor.

To further clarify issues of ownership and responsibility in Chinatown's alleyways, the following set of questions were asked of and answered by the City Attorney's Office (see page 120 - References.)

Q1: Are fronting property owners responsible for the sidewalk in front of their property or all the way to centerline of roadway?

A1: For accepted streets, fronting property owner is responsible for the sidewalk only. For unaccepted streets, fronting property owner is responsible for the sidewalk and to the centerline of the roadway.

Q2: What if the street is constructed in one level without curbs, gutters or roadway with one paving material and the sidewalk demarcate with bollards?

A2: Fronting property owners are responsible for legislated sidewalk width and is responsible for repairing the sidewalk with a reasonable slip resistant material such as gray concreate with a slip resistant finish. They are not expected to restore in kind if special paving material is used at sidewalk areas. (The vehicle code should be consulted for other restrictions.)

Q3: What if DPW proposes to consolidate sidewalks from each side of the alley into one wider sidewalk on one side of the alley?

A3: Widening the sidewalk requires legislation by the Board of Supervisors and a public hearing process. Fronting property owners must agree to accept responsibility for the wider sidewalk.

Q4: If DPW constructs special features such as bollards, benches, planters or other street furniture at the sidewalk, who is liable and

responsible for maintenance of these elements? What about trees?

A4: If DPW constructs these special features with public funds, DPW will be liable and responsible for these elements. If the public wants to construct such elements in front of their property, then they must apply for encroachment permits. Trees planted by DPW will be maintained by DPW unless an agreement is made as to who is responsible for maintenance. If an organization is amenable to accepting responsibility and liability for some or all of the street elements including trees, agreements can be prepared by the City Attorney's Office to obligate both public and private entities.

2. Placement of Public Art Projects

Background

The alleyways are opportunities to display the unique importance, historically and culturally, of the Chinatown community and more specifically of the role of Chinatown's alleyways. Placement of public art projects, depicting the community's history, is highly recommended as a component of the alleyway renovation work, particularly for the Phase I alleys (see page 31 - General Plan.) Because such a placement is considered an encroachment of a public right-of-way, similar to the placement of furnishings, trees, etc. on the sidewalk, some of the proposed special art projects may require city permits.

Players and Process

What qualifies as "works of art" are defined under the **Fine Art Guidelines** under the Downtown Plan (**City Planning Department**.) Proposed wall murals, engravings or imprints of special maps in the paving and "Footprints across Chinatown" may be considered as works of art.

The **San Francisco Art Commission** is the primary agency responsible for review and approval of all public art projects placed on the property of the City and County of San Francisco. The **Department of Public Works (DPW)** also must approve a regular maintenance and removal schedule prior to the installation of the projects (at this point, an encroachment permit may be required.) Other city departments' approval may also be required, depending upon the nature of the projects.

C. Cleaning Issues

Cleanliness is a major concern of the alleyway community. Without regular cleaning services, the livability of the alleys would quickly deteriorate. There are two related issues to consider under street cleaning services: (1) street sweeping and (2) garbage.

1. Street Sweeping

Background

The regularity and the consistency of street sweeping services is a critical issue for all of Chinatown's alleyways. Currently, as dedicated and accepted streets, all but one of the thirty-one alleyways under study legally have a right to regular city services (see page 83 - Maintenance Plan for details.)

Players and the Process

Street cleaning in the public right-of-way (sidewalks are to be swept typically by the ground floor occupant and if a property is vacant, by the owner) is provided by the **Department of Public Works' Bureau of Street and Environmental Services (BSES.)** All streets and alleys which have postings for street cleaning are provided with the service. Otherwise, mechanical cleaning (using a motorized vehicle) is not provided. (Six of the thirty-one alleyways have street cleaning signs.) All other streets and alleys, which are not cleaned mechanically must be manually swept, if they are dedicated and accepted streets.

Manual sweeping by the City is provided every day of the week from 7 am to 1 pm in Chinatown. However, not all alleys are swept at once. Anywhere from five to eight manual sweepers are assigned to the entire Chinatown area (not only alleys) daily, depending upon how many people are available from the **Welfare Department**. The Welfare Department supplies street sweepers throughout the City from a pool of welfare recipients needing job credit.

2. Garbage

Background

How the private and public garbage are contained and disposed of can make a significant difference to the appearance and health of the alleys. The presence of trash and litter in the alleys is very unsightly, but when the trash doesn't get picked up in a timely manner and is not properly contained, the conditions in the alleys can become even worse (see page 84 - Maintenance Plan for details.)

Players and the Process

The **Department of Public Works' Bureau of Street and Environmental Services (BSES)** regulates proper garbage containment and disposal. The **Public Health Department** also regulates proper trash containment, where public health concerns are involved. A warning is first given to perpetrators by field inspectors before a fine is placed. Only a few alleyways (5 total) currently have no littering or dumping signs posted with a cost for a fine listed for perpetrators.

The Chinatown garbage collection is serviced by the **Golden Gate Disposal Services**. The Golden Gate Disposal serves the Chinatown area every day of the week, but does not collect garbage from everyone everyday. There are three trucks which pick-up from the area at any given time. Most residents in the Chinatown area (including alleyways) get trash collected on Tuesdays. Commercial and large apartment complexes get multiple pick-ups depending on their needs. Commercial pick-up days vary, ie. Monday, Weds, Fri. or Tues. and Thurs.

The Golden Gate Disposal also provides free collection of large furniture and appliances twice a year if the service is requested, otherwise they do not and will not voluntarily schedule the pick-ups. This service is based upon customer request only.

D. Access

There are four types of access issues facing Chinatown's alleyways: emergency vehicles' access, limited vehicular access, street closures and disability access.

1. Emergency Vehicle Access

Background

Reasonable fire truck and ambulance access for emergencies are required of all alleyways. However, many existing alleyways are too narrow to provide the required standard widths for such access. About half of the alleys under study are less than twenty feet wide from property line to property line and are less than twelve feet wide from curb to curb.

Despite the existing conditions, provisions for emergency access is a consideration for the alleyway renovation projects. Emergency access, where it exists now, will continue to be provided while no additional provisions will be made for those without access currently. (Given the proposed renovation plans, meeting the Fire Department's standing policies should not be problematic. The same goes for other medical emergency vehicles, which typically require less clearance than fire trucks.)

Players and Process

Fire truck access is guided by the **Fire Department's** policies. The Fire Department typically looks for a standard curb to curb dimension of 14 to 20 feet clearance (without on-street parking) for large fire trucks. However, in Chinatown, exceptions to this standard are made since many of the alleys are not wide enough to accommodate it. Where there is no room (or the turning radius) to accommodate fire truck access, the Fire Department's policy is to access the alley environments from the main streets, and if feasible, from both ends of the alley. In this scenario, water lines are manually pulled through the alley, where a reasonable clearance for the pipes is required.

Generally in all cases, the Fire Department looks to maintain the current access of all alleys. The alleyway improvements should not decrease the accessibility of the alleys. For example, if Waverly Alley currently has clearance for fire truck access now, then it should have the same or improved access after the renovations. The Fire Department's policy is to review the alleyway projects on a case by case basis internally and with the Interdepartmental Staff Committee on Traffic and Transportation (ISCOTT), where the Fire Department is a representative.

2. Limited Vehicular Access

Background

In one alley, Jack Kerouac, the *Alleyway Master Plan* proposes a limited access of the alley to delivery trucks only; this is to ensure that pedestrian access is given a higher priority to cars (see page 61 - Renovation Plan.)

Limited vehicular access entails that only delivery vehicles are allowed to enter the alley and only for the time required of deliveries. Likewise, parking in the limited access alleys will be permitted for drop-off purposes only.

Players and Process

The **Department of Parking and Traffic (DPT)** is the primary agency responsible for processing a limited vehicular access permit.

Required DPT Process:

- Petition from the community (majority - 51% - of property owners/ residents/ merchants) to propose a limited vehicular access to DPT.
- A review at the Interdepartmental Staff Committee on Traffic and Transportation (ISCOTT.)
- First public hearing at the DPT Staff Review
- Second public hearing at the DPT Commission(Consent Calendar Item)
- Legislation by the Board of Supervisors
- Legislation signed by the Mayor

3. Street Closures

Background

Three alleys, Hang Ah, Ross and Spofford are to be considered for street closures from 10 a.m. to 4 p.m (see pages 42, 45 and 48 - Renovation Plan.)

A street closure entails closing the alley to vehicular traffic, except for emergency vehicles. (Fire Department approved removable bollards operable by key will be utilized as barriers at both ends of the alleyway, allowing emergency vehicles' access. The key would be made available to a resident or a merchant responsible for the street closure operation.) Pedestrian access would be maintained as is while delivery trucks will be allowed to enter the alley only during the early morning hours before 10 a.m. and in the late afternoon hours after 4 p.m.

A street closure effectively signifies an alley to be primarily pedestrian first. For Ross and Spofford alleys, a street closure makes sense, because they are narrow lanes and are heavily used by pedestrians. For Hang Ah, which is primarily a pedestrian oriented alley, closely associated with adjacent recreational uses, street closure is also highly appropriate. Vehicles in these alleys threaten pedestrian safety and do not enhance the alleys' primary functions.

Players and Process

The **Department of Parking and Traffic (DPT)** is the city agency responsible for processing street closure permits.

Required DPT Process:

- Petition from the community (majority - 51% - of property owners/ residents/ merchants) proposing street closure to DPT.
- A review at the Interdepartmental Staff Committee on Traffic and Transportation (**ISCOTT**.)
- First public hearing at the DPT Staff Review
- Second public hearing at the DPT Commission (Consent Calendar Item)

- Legislation by the Board of Supervisors
- Legislation signed by the Mayor

4. Disability Access

Background

Disability access in the alleys is a requirement that all renovation projects must accommodate in order to obtain city approvals to build and to retain the City's "acceptance" of the alleys.

By access, the law requires that there be reasonable access for persons with disabilities, i.e. wheelchair access, hearing impaired and sight impaired. Specific requirements for sidewalk widths, paving materials, and signage exist as measurements for disability access.

Majority of Chinatown's alleyways currently cannot meet many of the disability requirement standards due to physical dimension constraints. For instance, a requirement of sidewalks on both sides of the alley cannot be accommodated in the majority of the alleys because there isn't the space. It is, however, a goal of the *Alleyway Master Plan* to see that reasonable disability access be achieved, where possible (see below.)

Players and Process

Disability access requirements are determined by the **Americans with Disabilities Act (ADA)** \ and **Title 24** of the California Building Code, and governed by the California Accessibility Regulations under the Office of the State Architect.

In San Francisco, accessibility standards are reviewed on a project by project basis under the **Disability Access Coordinator's Office**, which is under the Department of Public Works. (Because most of Chinatown's alleyways cannot meet the optimum disability standards as required, a discussion with the Disability Access Coordinator's Office should occur prior to implementation to determine the "reasonable disability access" that can be achieved in each of the alleyways.)

E. Parking

1. On-Street Parking

Background

In a highly congested Chinatown, there is no question that on-street parking is a valued commodity to the Chinatown community, especially to the businesses. For this reason, many have traditionally opposed the removal of any on-street parking. Recently, however, both merchants and residents have begun to question the benefit of on-street parking, particularly in small alleyways. Existing parking spaces, because there is typically a low turn over rate, do not benefit either residents or merchants. During community meetings, many community members discussed their disenchantment with the way on-street parking create more of a visual and physical barrier in alleys than providing a benefit.

For aesthetic and public safety reasons, the *Alleyway Master Plan* proposes to remove on-street parking at two alleys: Beckett and Wentworth Alleys (see pages 35 and 55 - Renovation Plan.) Beckett and Wentworth are narrow alleys and the presence of on-street parking reduces them to even narrower right-of-ways, making it difficult and sometimes impossible for through traffic, especially when there is double parking.

Players and Process

The **Department of Parking and Traffic (DPT)** is the city agency which implements and enforces on-street parking. Proposals to change existing on-street parking requires a similar DPT process as applied to other traffic access improvements.

Required DPT Process:

- Petition to DPT from the community (majority - 51% - of property owners/ residents/ merchants) proposing removal or addition of on-street parking.
- A review at the Interdepartmental Staff Committee on Traffic

and Transportation (**ISCOTT.**)

- First public hearing at the DPT Staff Review
- Second public hearing at the DPT Commission(Consent Calendar Item)
- Legislation by the Board of Supervisors
- Legislation signed by the Mayor

F. Street Signage

A goal of the *Alleyway Master Plan* is to bring more visibility to Chinatown's alleyways and to highlight them as inviting, interesting and historically significant places. Signage to where the alleys are located in the community is one of the *Alleyway Master Plan's* proposals to achieve such a goal.

1. Street Name and Directional

Background

There are two types of street signs proposed under the *Alleyway Master Plan*: name and directional signs (see pages 31 and 32 - Renovation Plan.) The name signs identify the alley by name and the directional signs lead pedestrians to the key or more historically significant alleys.

Players and Process

The **Street Signs Division under the Department of Parking and Traffic (DPT)** is responsible for the implementation of street signs throughout the City. The DPT Commission reviews proposals and grants approvals for street signage while the Street Signs Division makes the signs.

DPT is receptive to the idea of alleyway signs being unique. There are a number of precedents in the City, i.e. Yerba Buena and Mission Street, where distinctive street signage have been used. DPT plans to work with the community on the design of the alleyway signage.

G. Enforcement

Enforcement of unlawful activities, whether it is littering or illegal parking, is critical for the viability of Chinatown's alleyways. In this section, enforcement issues are discussed under police, parking and public health.

1. Police, Parking and Public Health

Background

Criminal acts, robberies and assaults in the alleys, undercut public safety greatly in Chinatown's alleyways. Because alleys are usually small, dark and not very visible, they are areas of opportunity for criminals. Crimes perpetrated on individuals have been cited a number of times by alleyway users.

Illegal parking, littering or garbage dumping are also issues where renovation plans alone cannot entirely eliminate problems in the alleys. Proper and consistent enforcement are needed to ensure the longevity of improvements implemented (also see page 90 - Enforcement Plan.)

Players and Process

The **Police Department** enforces and can cite criminal activities. The police can also cite a number of other municipal and health codes. Currently (as of Dec. 95), there are only three beat patrol officers (out of the approximately 100 officers stationed at the Chinatown/North Beach Station) walking the streets of Chinatown. The number of officers assigned to patrol the Chinatown area is directly related to the number of complaints the Police Department receives. Since the Department has historically reviewed very few complaints from the Chinatown community, only three officers are assigned.

Other than the Police Department, the **S.A.F.E Project**, Safety Awareness for Everyone, a community crime prevention program sponsored by the San Francisco Police Department, should be contacted for crime related reports at 415 673-SAFE.

Appendix - Enforcement

Parking enforcement is administered by the **Department of Parking and Traffic (DPT.)** Only two officers are assigned to the Chinatown area including alleyways (as of Dec. 95.) One officer is assigned to enforce the meter parking and the other is responsible for the 1 and 2 hour limit parking. Usually each officer patrols an area of 10-15 block radius.

Litter control is implemented through the **Public Health Department** and the **Department of Public Works' Bureau of Street and Environmental Services (BSES.)** Any illegal garbage dumping must first be reported to these city departments. Once a complaint is filed, an investigation will follow with due process. Non-compliance, after a warning, is usually met with penalty fees. Field inspectors from both the Public Health Department and BSES are assigned to monitor the streets and alleys of Chinatown.

H. City-wide Coordination

The *Chinatown Alleyway Master Plan* Report, as discussed earlier, is the guiding document for the alleyway renovation projects. As a guiding document, which outlines the desired policies, design guidelines and the scope and breadth of work to be completed, it outlines the detailed coordination that will have to be completed between the city departments and between the City and the community.

Coordinated efforts are critical to successfully implement the proposed plans. While the Chinatown Community Development Center, as the initiator of the improvement program and the author of this report, shall continue its role as the primary coordinator, it will take everyone's efforts, the City's and the community's, to realize the long term success of the renovation projects. In this section, two specific coordination efforts are discussed: (1) coordination between the Downtown Streetscape Plan, an existing city planning document, and the efforts of the *Alleyway Master Plan* and (2) coordination required between the various city departments in the implementation of the renovation projects proposed here.

1. The Downtown Streetscape Plan

Background

The Downtown Streetscape Plan, authored and adopted by the San Francisco City Planning Department and funded by the San Francisco Transportation Authority, is a document that provides the design and policy visions for the downtown pedestrian oriented streets, including alleyway (see page 120 - References.) In this sense, it is very much like the *Chinatown Alleyway Master Plan* except that it covers a different geographic region and encompasses a larger breadth of scope, beyond alleyway environments.

Because of the similarities in intention and scope, there is much that the *Alleyway Master Plan* projects can learn from the

research and work that has already been completed on the Downtown Streetscape Plan. Particularly for issues of city-wide impact, i.e. street lighting and city codes requirements, the two projects could benefit greatly through close coordination and communication.

Players and Process

The **City Planning Department** is the primary coordinator of the Downtown Streetscape Plan.

2. DPW Projects Coordination

Background

A number of streetscape related projects, i.e. sewer work, water pipes replacements, street repaving, lighting projects, etc., are planned for Chinatown in the next five to ten years, similar schedule as the *Alleyway Master Plan* (see page 77 - Implementation Plan.) Coordination of these various projects is critical for a smooth and efficient implementation of the alleyway renovation projects.

Players and Process

The **Committee for Utility Liaison on Construction and Other Projects or CULCOP** is responsible for the coordination of city-wide utility and infrastructure improvement projects. CULCOP consists of representatives from the various city departments: DPW engineers, Sewers and Utility Departments, the Fire Department, etc. Any streetscape project that may require coordination, should be reported to CULCOP and must be eventually "cleared" by the committee before a construction project could commence. It may take up to two years for a project to clear CULCOP. In the monthly CULCOP's "Street Clearance Report," the schedule and project coordination of various city street projects are listed.

Alleyway Master Plan Implications

- **CULCOP'S List:** The alleys under the *Alleyway Master Plan*, specifically the priority alleys under Phase I and Phase II, should be listed for CULCOP's clearance as soon as the

Appendix - Coordination

Alleyway Master Plan is reviewed and approved by the community and the affected city departments.

- **Sewer Projects:** It is critical that the *Alleyway Master Plan* renovation projects, especially the Phase I projects, are closely coordinated with the on-going and upcoming sewer line replacement work. The sewer projects should commence and end before the beginning of the construction for the alleyway renovation projects.
- **Water Department Projects:** Water line replacement work, similar to the sewer work, should commence and end before the start of any of the alleyway projects.
- **PG&E Gas Line Replacement Projects:** PG&E currently is conducting gas main line replacement work throughout Chinatown. Parts of the project have been completed in major streets and in a number of alleyways. However, the work is on-going and is expected to be completed in the Chinatown core area by the end of 1997.

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- Adopt-An-Alleyway Youth Project (AAA)
- Committee for Better Parks and Recreation (CBPRC)

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